

Shaw, Mt. Vernon Square, Blagden Alley/
Naylor Court Area

Conceptual Streetscape

Design Guidelines

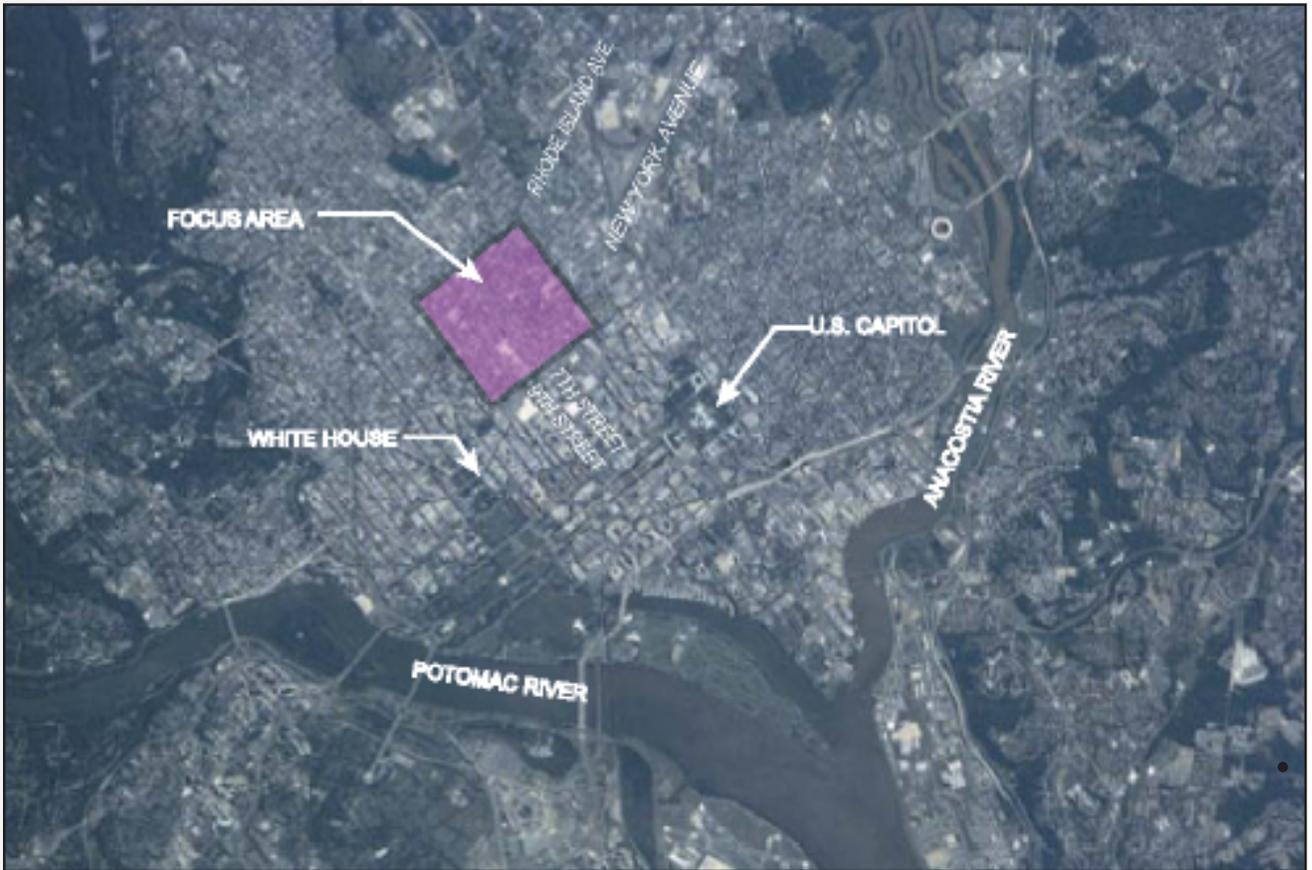
for the
7th and 9th Street Corridors
from Mt. Vernon Square
to Rhode Island Avenue



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What is smART streets?



smART streets is a fully-funded community project that will enliven the pedestrian environment in neighborhoods around the Convention Center through public art and good urban design. Funded through the District of Columbia Department of Transportation's TEA-21 program, smART streets will:

- Install art at three important neighborhood locations.
- Develop design concepts to guide future planned streetscape improvements.

This initiative is one important outcome of the Washington Convention Center Authority's (WCCA) \$1 million Historic Preservation Fund. The Fund was established as a result of the Convention Center Environmental Impact Study. The Fund is administered by the NTHP.

The Fund also was used for exterior rehabilitation grants, a Heritage Trail, and Historic district street signage.

I. Introduction

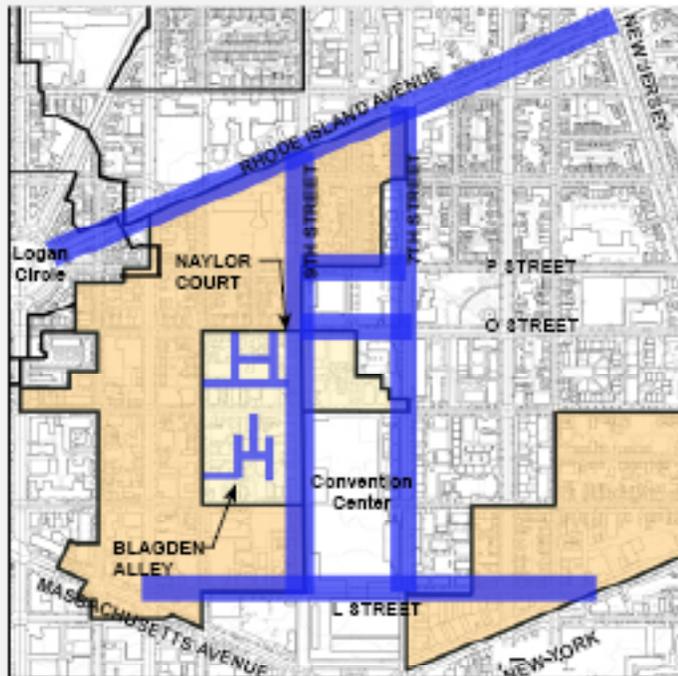
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Mount Vernon Square

Figure 1a: 7th Street NW at New York Avenue

About The Conceptual Streetscape Design Guidelines

The following conceptual streetscape design guidelines offer a variety of ideas and concepts for improving the quality and character of the major streets that connect the Shaw, Mt. Vernon Square, and Naylor Court/Blagden Alley Historic Districts and surrounding areas together. The purpose is to reinforce the Convention Center Area Strategic Development Plan's goals, to reinforce a clear street hierarchy that differentiates between residential and commercial streetscape and to draw from the existing character while accommodating contemporary business requirements. These guidelines will help guide future design decisions by the District's Department of Transportation (DDOT), and to assist property owners in developing related streetscape improvements by:

- Improving the visual identity and pedestrian safety of the area's major commercial corridors
- Creating a pedestrian friendly environment that will encourage people to walk to area businesses from downtown
- Respecting and preserving historic district and character-defining features of the area and complementing the new Heritage Trail
- Helping DDOT to develop site specific design solutions and to prioritize future design projects
- Mitigating negative impacts of the DC Convention Center to the area

The corridors and streets that are the subject of the guidelines are (Figure 1a):

- 7th and 9th NW, bounded to the south by Mount Vernon Square and to the north by Rhode Island Avenue
- O and P Streets between 7th and 9th Streets
- L Street NW between New York Avenue and Massachusetts Avenue
- Rhode Island Avenue NW between New Jersey Avenue and Logan Circle
- Blagden Alley and Naylor Court

Because the guidelines are designed to focus on issues related to the design of the streetscape and pedestrian environment, a number of related planning and design issues, that are addressed in other planning and guideline reports, as outlined in the appendix (pg 49), are not addressed here such as:

- Changes to existing zoning
- Improvements to traffic and traffic circulation

I. Introduction

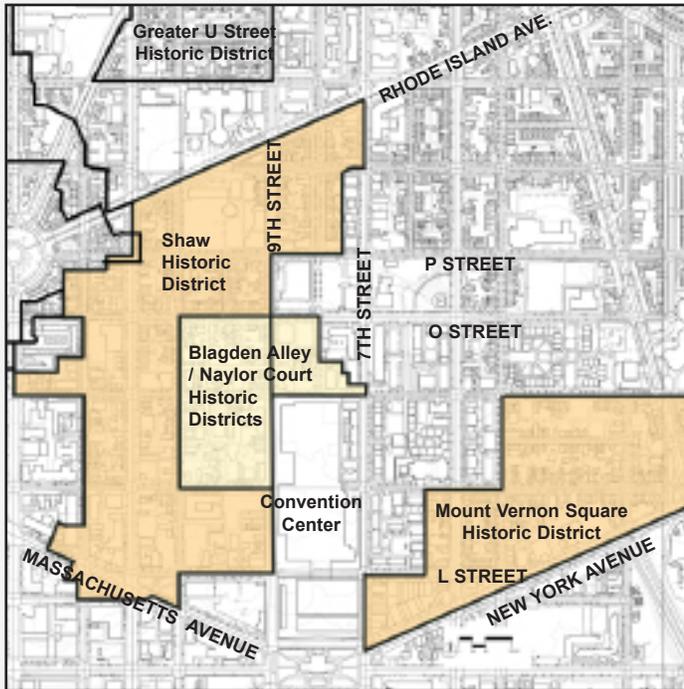


Figure 1b: Historic District Map



Figure 1c: Historic District Signage

- Guidelines for improving building facades

Background

In 1997, an Environmental Impact Study for the new Convention Center determined that its construction would have a negative effect on the surrounding historic mixed-income neighborhood. Some of the impacts cited included parking congestion, truck and other convention center traffic, large building scale relative to the size of buildings in the surrounding area, potential displacement of local minority businesses, and potential displacement of low-income minority residents. These impacts contrasted with the new Convention Center's potential to generate new jobs and retail demand that the neighborhood might not experience otherwise.

As a way to mitigate the negative impacts, the Washington Convention Center Authority, (WCCA) established a \$1 million Historic Preservation Fund. The National Trust for Historic Preservation's (NTHP) Community Partners Program serves as the administrator of the fund and related programs under a cooperative agreement with WCCA.

To date, approximately \$750 thousand have been used for the exterior rehabilitation of buildings under a loan and grant program targeted primarily for properties within the Shaw, Mt. Vernon and Blagden Alley/Naylor Court historic districts, along with other structures in the surrounding area.

The remaining funds have been used for the following:

- Creation of two historic districts--Shaw Historic District and Mt. Vernon Historic District (Figure 1b).
- Implementation of historic district street signage designed by an advisory board including the DC Historic Preservation office, DDOT, Cultural Tourism DC, and participants from the historic districts. Measuring two feet in diameter, the signs are located at key intersections along the historic district boundaries (Figure 1c).
- Establishment of a Heritage Trail and related trail signage that was completed in late 2005. (Figure 1d)
- Completion of a Phase I Public Art Study in the spring of 2002 resulting in the identification of 21 art opportunity sites including several priority sites, and the need to develop conceptual streetscape design guidelines for the major commercial corridors in the area. As a result of the Phase I Public Art Study it was

I. Introduction



Figure 1f: Carter G. Woodson Park



Figure 1g: Kennedy Recreation Center

community-wide and area-wide planning issues and opportunities. The previous recommendations that have helped to shape these guidelines are:

Convention Center Area Strategic Development Plan

- Study the traffic patterns on 6th, 7th, 9th and 11th streets to determine how best to move vehicles and pedestrians through the neighborhood
- Improve appearance of gateway intersections
- Extend the retail corridor from the Convention Center to the Uptown Destination district
- Encourage a mixed-use development on the O Street Market block that is the focal point for the retail corridor
- Improve storefront facades along 7th & 9th Street
- Rezone 7th Street between O and Rhode Island Avenue, 9th Streets to require that new development include ground floor retail (approximately 200,000 sq. ft.)
- Locate Convention Center headquarters hotel at 9th and Massachusetts Avenue as the southern anchor of the retail corridor (and gateway corridor)

Shaw Main Streets Design Committee Work Plan-- Community Vision

- Create visually appealing streetscapes along the 7th and 9th Street Corridors with 0% vacancy and 100% affordability
- Clean, crime-free streets, tree-lined streets, no trash anywhere
- Beautiful brick sidewalks and walkable streets, 100% Jefferson herringbone sidewalks
- Lots of street lights to enliven the night
- Use design to unify the disparate elements of the corridors
- Emphasize the historic and cultural heritage of Shaw
- Create a safe, appealing, walkable, and vibrant commercial corridor that is inviting to residents and visitors alike.

Strategic Neighborhood Action Plans (SNAPS)

- Develop tree plan
- Renovate and provide additional planting on the grounds of the Watha T. Daniel Library
- Install and maintain additional trash cans,

I. Introduction

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Strategic Neighborhood Action Plans (SNAPS)

- Develop tree plan
- Renovate and provide additional planting on the grounds of the Watha T. Daniel Library
- Install and maintain additional trash cans, provide amenities that active use
- Renovate large alleys to encourage retail and residential development
- Extend streetscape improvements into adjacent neighborhoods and commercial districts. Create outdoor performance areas
- Stimulate development of commercial districts on 7th and 9th Streets
- Conduct studies and develop strategies to increase parking on residential streets for area residents
- Develop design guidelines to integrate residential, commercial and public development and encourage 1st floor retail on 7th and 9th Streets
- No loss of on street parking for residents

NTHP Commercial Corridor Development Plan (7th and 9th Streets)

- Develop streetscape guidelines including lighting, landscaping, signage, furniture, and sidewalk elements, in order to ensure consistency in appearance and materials throughout the corridors
 - Expand and increase efforts to protect and monitor historic properties
 - Develop design guidelines for the corridors to encourage appropriate historic rehabilitations, new construction compatibility, high quality design, and minimal "facadectomies"
 - Develop greater heritage tourism attractions
 - Expand efforts to educate new and existing merchants and property owners about historic preservation
- Other studies and plans reviewed in the development of this guideline are:

Thrive: A Guide to Storefront Design
DDOT Plans and Guidelines
Historic Preservation Guidelines
Downtown Action Plan
Uptown Destination District Strategic Development Plan
Mount Vernon Master Plan
K Street Redevelopment Plan
City Vista

II. The History of the Area



Figure IIa: Historic O Street Market



Figure IIb: Old Shaw Jr. High School at 7th & Rhode Island Avenue

The 7th and 9th Street commercial corridors have a rich and unique history that began in the mid-nineteenth century. Since their early development, they have hosted a wide variety of merchants, industries, and businesses that have provided a myriad of goods and services to surrounding residential areas and to the city as a whole.

Initial development of the Shaw/Mt. Vernon Square area began in the 1830's. During the ante-bellum period, the neighborhood became one of the most densely populated residential areas in the city, due to its proximity to the downtown business district. Commercial development quickly followed, spurred by the construction of the Northern Liberties Market at Mt. Vernon Place in 1842 and the presence of 7th Street, the first paved road in the District and the primary transportation route for northbound travel.

During the Civil War and Reconstruction, an influx of military personnel, government workers, and African-Americans created a major housing shortage in the city and a subsequent construction boom. Thousands of residential dwellings were erected in the area and commercial development intensified as new businesses were established to meet the needs of the growing resident base. By the 1890's streetcar service had been installed on 14th, 11th, 9th, 7th, and 4th Streets, which furthered the growth and expansion of the neighborhood. During this period, the population was primarily middle and working class and included both African-American and white residents. In the early 1900's, as transportation improvements created opportunities for development to the north, middle-income white residents began moving out of the area which then became predominantly African-American.

Over the next several decades, the neighborhood flourished as business and commerce continued to grow along U, 14th, 9th and 7th Streets. Dozens of new African-American businesses, fraternal organizations, religious institutions, and entertainment facilities were established. The area became the center of social, cultural, and economic life for the District's African-American community and home to some of the city's most prominent citizens. By the early 1940's Shaw/Mt. Vernon Square had become a thriving, prosperous neighborhood.

After World War II, desegregation and the opening of new housing markets prompted many middle-class African-Americans to leave. Single-family homes were converted to multi-family rental dwellings and the density of the area increased. The businesses along the commercial corridors

II. History of the Area



Figure 11c: *Historic Architecture on M Street*



Figure 11d: *Historic Carter G. Woodson House*

began to suffer as a result of losing a large segment of their traditional local customer base and many of them were forced to close. By the late 1950's, the neighborhood had begun to decline.

The problems that emerged in the post-World War II period were exacerbated by riots that erupted after the assassination of Dr. Martin Luther King, Jr. in 1968. Countless commercial buildings were damaged and several were destroyed entirely. Many of the surviving businesses left the area or closed their operations altogether. Throughout the 1970's and 1980's middle-income residents continued to move out of the community causing a continuation of the area's economic decline, and the continued disinvestment of the area's commercial corridors.

Over the last 15 years however there have been promising signs of renewal and revitalization throughout the area. In 1991, metro rail service was extended into the neighborhood, which increased the accessibility and market reach of local businesses, improved the mobility of residents, and enhanced the area's overall livability. Significant redevelopment initiatives have taken root along 14th Street and U Street, and both corridors are beginning to recapture their former health and vibrancy. The redevelopment of the Kennedy Playground helped to focus neighborhood activities. And the addition of two historic districts in 1999 that encompass the areas immediately to the east and west of the new Convention Center helped to stabilize the area's character.

Most recently, an increasing demand for inner-city housing has created significant increases in both the rehabilitation of single-family homes and new residential construction. The increase in residential activity has in turn expanded the need for and establishment of residential-serving retail.

Along with the completion of the new Washington Convention Center, which has had a major impact on the area, these housing and retail opportunities are coupled with difficult challenges for the area. Increased development in could impact significantly on the area's existing businesses, historic character, and residential complexion. In addition, the retail and service needs generated by convention attendees and tourists will need to be balanced with those of local residents and their shopping and related service needs.

Neighborhood History section has been developed from excerpts taken from the "Commercial Corridor Development Plan for 7th and 9th Streets", Presented by The National Trust for Historic Preservation, December 2001

III. SWOT Analysis (strengths, weaknesses, opportunities, and threats)

In order to better understand the pedestrian and streetscape issues facing the general area, and specifically the study streets, a series of walking tours, community meetings, and photographic analysis were conducted. The information gained from these observations and assessments is outlined in the following listing of Strengths, Weaknesses, Opportunities and Threats (SWOT):



Figure IIIa: Buildings on Rhode Island Ave & Logan



Figure IIIb: Kennedy Recreation Center

Area Strengths

- **Strong "sense of place"** is derived from the area's unique social and cultural history and architectural character. (Figure IIIa)
- **Located adjacent to and within walking distance of downtown** and wraps around the New Convention Center.
- **Good residential and commercial building stock** highly adaptable for revitalization and renovation.
- **Economic momentum building** with increasing demand for residential and residential-serving retail space.
- **Good access to ground floor retail uses** resulting from a traditional street and sidewalk pattern with significant portions of the 7th and 9th Street corridors lined with existing or potential retail uses.
- **Good access to Metro, Metrobus, and city transit services**--Shaw/Howard University Metro station at 8th and R Streets; Mt. Vernon Square/Convention Center Metro station at 7th and M Streets; Major bus routes on 7th & 9th Streets and Rhode Island Avenue; Downtown Circulator bus connects the New Convention Center with downtown destinations, Georgetown and Union Station; Bicycle lanes on R Street connecting to the Shaw Metro Station and proposed bicycle lanes on 7th Street.
- **7th Street designated in the city's "Great Streets" program** and is a major north/south connection between downtown and the proposed Uptown Destination District; Howard University; and Silver Spring, MD.
- **Rhode Island Avenue is a significant gateway to the city** and a major east/west connector between downtown (via historic Route #1) the University of Maryland, the Capital Beltway and Prince George's County, MD.
- **Active community organizations** have a desire to improve the quality and character of area streets and sidewalks (Figure IIIb).

III. SWOT Analysis



Figure IIIc: *Empty Corner Buildings and Lots*



Figure IIIId: *Narrow Sidewalks on 9th Street*



Figure IIIe: *Inconsistent Streetscape; Store Loading*

Area Weaknesses

- **Inconsistent streetscape treatment results in a lack of a visual and functional cohesiveness**--varying and inconsistent sidewalk paving patterns and materials, crosswalk design, tree box treatment, varying signage, newspaper boxes, and street lighting creates visual clutter and a poor public image (Figure IIIc)
- **Sidewalks in front of retail buildings are not designed to promote sidewalk activity--The lack of sidewalk amenities** such as sidewalk cafes, public art, street furniture treatments, attractive street lighting present an uninviting quality--corner stores and retail establishments are not taking advantage of wide sidewalks and potential for attracting pedestrian business
- **Paving maintenance and repair** appear implemented with less expensive and less attractive materials creating a patchwork appearance
- **Large buildings such as the Convention Center are out of scale** with surrounding neighborhood scale and character
- **Cobra head street lights located north of n street on 7th and 9th create a harsh uninviting pedestrian image** as compared with the washington globe fixtures located below n street
- **Gas stations at the Rhode Island and New Jersey Avenues present a negative image** and detract from the potential of creating a beautiful gateway to the Shaw neighborhood
- **Extremely narrow sidewalk on the west side of 9th Street between Mt. Vernon Square and N Street not designed to accommodate large pedestrian loads**--some sections of the pedestrian right-of-way measure less than three feet from curb to building entrance steps--these narrow areas often further interrupt pedestrian traffic with telephone poles, light poles or parking meters (figure IIIId)
- **An unattractive industrial image given to 9th Street** by the Giant grocery blank walls, surface parking lot, and tractor trailer loading docks (figure IIIe)

III. SWOT Analysis



Figure IIIf: Historic O Street Market Site



Figure IIIg: Gateway Triangle at L Street & New York



Figure IIIf: Entrance Plaza to Mount Vernon Square/Convention Center Metro Station.

- **Poor quality of commercial signage adds to an uninviting image** and lack of consideration of the historic character of many retail storefronts
- **Numerous unattractive sidewalk grates in high pedestrian areas**, especially along 7th Street add to poor sidewalk use and image
- **Mature trees, especially along Rhode Island Avenue, buckle sidewalks** to a height of six inches or more creating significant pedestrian hazards--flattening sidewalks at the base of mature trees will dramatically affect the health and life of the trees
- **Trash, poorly kept empty lots, and graffiti detract from the visual quality of the area** and fosters a perception of poor security and neglect
- **Tour buses and vehicles appear to be the dominant transit mode** rather than easy pedestrian access and inviting pedestrian environments

Opportunities

- **Historic districts and buildings create a unique neighborhood image** that can enhance the area's attractiveness to pedestrians coming from downtown and the New Convention Center
- **The historic O Street Market/Giant site offers an exceptional an opportunity for creating a neighborhood village or town center--a central place that could focus public attention toward the area** (Figure IIIf).
- **7th and 9th Streets are major vehicular north- south connectors that require pedestrian improvement** in order to fulfill their potential as major pedestrian pathways that connect downtown to the area's historic resources and retail establishments including access to U Street and the Uptown Destination District
- **The current sidewalk and street patterns offers numerous opportunities** for additional green spaces, plazas and parks, sidewalk cafes, shaded sitting areas, and public art sites that would enhance the pedestrian experience and the area's attractiveness (Figure IIIf).

III. SWOT Analysis



Figure IIIg: Neglected Corner Treatment



Figure IIIh: Parking Lots Adjacent to the Roadways



Figure IIIi: Neglected Corner Treatments

- **Metro Station plazas provide opportunities for creating vibrant art filled gateways to the area** (Figure IIIg)
- **New development and the new Convention Center should promote increased interest in the area** and access to local businesses
- **Kennedy Recreation Center provides a central place and public venue for community based initiatives and programs--** located next to the O Street Market, the park adds to the image of a central place in the heart of the area
- **Rhode Island Avenue presents an opportunity to create a majestic boulevard as a gateway to the area--**a greened, art filled, decoratively landscaped entranceway to the city

Threats

- **Actual and perceived threat of crime and lack of public safety within the area** threatens the pedestrian use of the area for residents and visitors
- **Poorly lit, neglected, visually uninviting building facades** add to a blighted image especially along 7th and 9th Streets (Figure IIIh.)
- **Competition from nearby development** in the downtown area to the south and the Uptown Destination District and U Street to the north present a threat to developing businesses in the area
- **Increasing property value** threatens dislocation of family and neighborhood resident-owned retail and business establishments that will threaten one of the area's characteristics that people find attractive
- **Potential for more large-scale development** within and directly adjacent to the area that will threaten its historic character and traditional retail street image unless carefully designed and developed to achieve pedestrian and neighborhood goals (Figure IIIi).
- **Mature trees that buckle sidewalks** limit sidewalk accessibility and safety. Sidewalk repairs that damage roots threaten the life of the tree. (Figure IIIj).

IV. Street Character

This section of the Conceptual Streetscape Design Guidelines describes the character or nature of the streets in an effort to better understand the issues that should be addressed by making improvements to the streetscape and pedestrian environment.

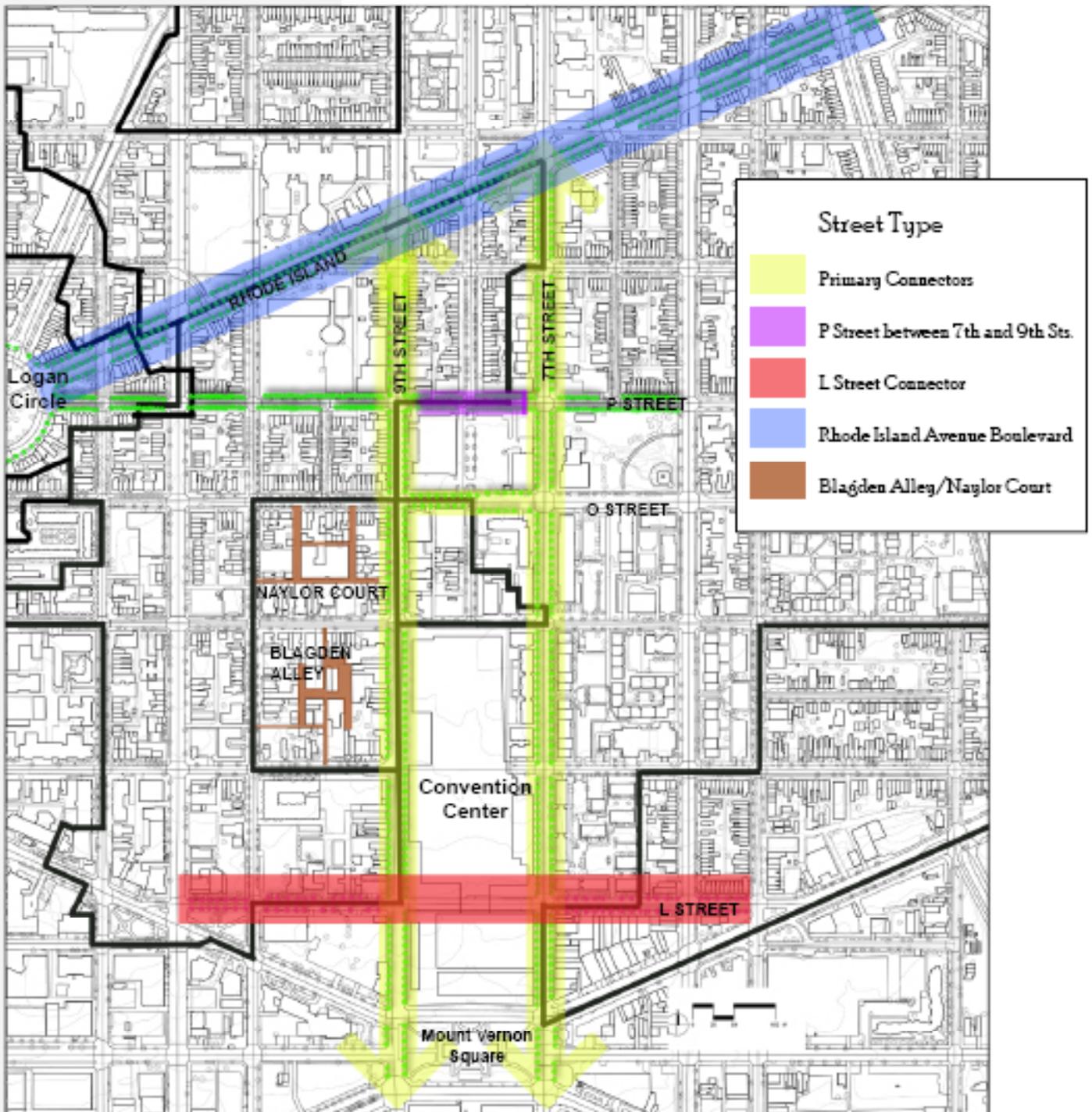


Figure IVa: Character Area Map

IV. Street Character

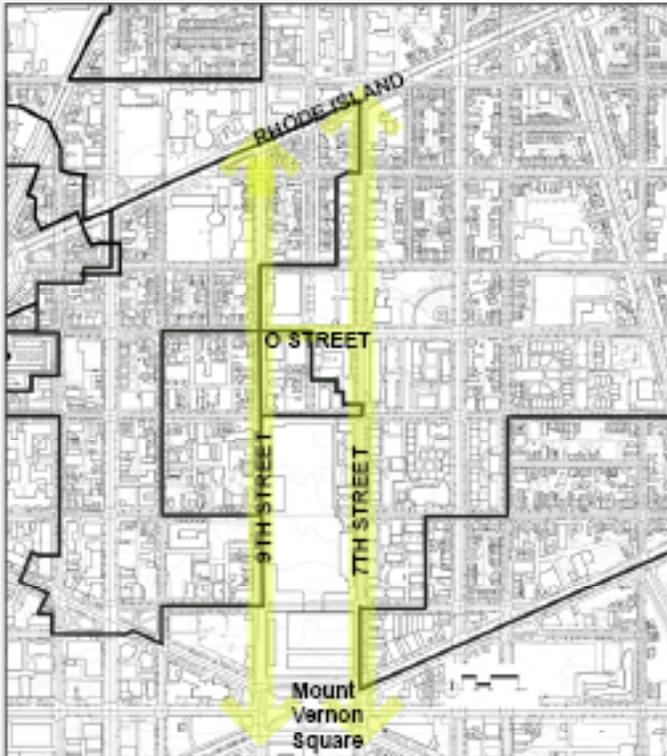


Figure IVb: Location Map



Figure IVc: Convention Center facing existing historic store fronts

Primary Connectors-7th, 9th, and O Streets

7th and 9th Streets form the infrastructure backbone that ties the area together. They are also the primary retail streets that house neighborhood stores and resources. Their role is to:

- Link the area to centers of activity located to the north and south.
- Act as a zipper or seam that brings the historic districts located to the east and west together.
- Create a unified streetscape image for the area and a safe pedestrian environment for walking to and from the area.

As outlined in the introduction, a number of community and neighborhood plans have identified 7th and 9th Streets as important commercial corridors that connect the Shaw and Mt. Vernon Square neighborhoods to the downtown and the Mt. Vernon Triangle area located to the south, and the Uptown Destination District and U Street/Florida Avenue corridor to the north.

7th Street in particular is important since it connects the area with Howard University and further northward to Silver Spring in Maryland. Furthermore, it has been designated by Mayor Williams for inclusion in the District's "Great Streets Program" that is intended to implement new sidewalks, street lighting, and paving in order to "improve the physical setting, reduce crime, attract private investment and neighborhood businesses, create housing opportunities and jobs, and boost the quality of life."

Several key issues that characterize the Primary Connector Streets are:

- 7th Street between Mt. Vernon Square and Rhode Island Avenue presents an inconsistent visual and functional character. Due its wide variety of land uses and building types, and an inconsistent use of streetscape elements the street does not currently appear as important, as a primary connector street. Rather than one character, the street currently is divided into four segments. .

At the southern end, the large scale New Convention Center dominates one side of the street in contrasted to the historic store fronts

IV. Street Character



Figure IVd: 7th Street Low Rise Housing



Figure IVe: Intersection of 7th & N Streets marks the change in existing streetscape character

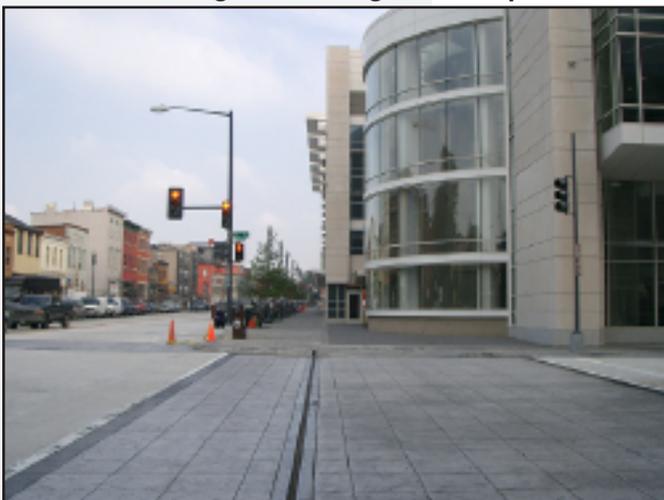


Figure IVf: 9th Street Building Character

on the west side of the street that most likely will be topped with residential/office uses. (Figure IVc).

Moving northward, the street is notable for fairly dense low- and high-rise apartment buildings that are set back from the sidewalk with wide planting strips creating a residential character (Figure IVd).

The center of the street, between Mt. Vernon Square and Rhode Island Avenue, is characterized by the historic O Street Market, Giant Foods, and the Kennedy Playground.

Toward Rhode Island Avenue the street is flanked by low-rise housing and single story retail stores.

The most significant difference in the quality of the street environment may be observed at 7th and N Streets. To the south of N Street the streetscape includes the consistent use of a new sidewalk paving pattern, street trees with metal tree grates, and new double-head Washington Globe street lights. In contrast, the streetscape located to the north of N Street is characterized by large-scale cobra head street lights, and the inconsistent use of sidewalk paving materials, plant materials, trees, and tree box treatments. (Figure IVe).

- **9th Street between Mt. Vernon Square and O Street** presents a challenging streetscape and potentially unsafe pedestrian environment.

The east and west sides of 9th Street between Mt. Vernon Square and N Street are extremely different in their design and quality. The east side is characterized by the wide sidewalks and cool colors that surround the New Convention Center. In stark contrast, the west side of the street is characterized by historic brick retail storefront facades and extremely narrow sidewalks that measure less than three feet in some locations making it impossible for large numbers of pedestrians to use the sidewalk safely. (Figure IVf). In many places the pedestrian right-of-way is also obstructed by telephone poles, sign poles, parking meters, and wide front steps leading to secondary story levels in the historic buildings.

IV. Street Character



Figure IVg: *Narrow Sidewalks*



Figure IVh: *O Street*



Figure IVi: *Former O Street Market*

DDOT is currently conducting a vehicular capacity study for 9th Street that, depending on the results, may or may not allow for widening the sidewalk on the west side of the street. The ability to widen the sidewalk will likely affect how the ground floor retail stores in the historic structures will be used by conventioners and other pedestrians walking to local businesses in the area from the Convention Center (Figure IVg).

- **O Street between 7th and 9th Streets** creates a critical pedestrian link between 7th and 9th Streets. With the likely redevelopment of the O Street Market site, bounded by O and P Streets, 7th and 9th Streets, O Street may potentially become its new front door. Due to its prominence, a variety of opportunities for streetscape enhancements are possible including including south facing sidewalk cafes, street level stores and shops, public art sites and gateway elements (Figure IVh & IVi).

IV. Street Character

P Street between 7th and 9th Streets

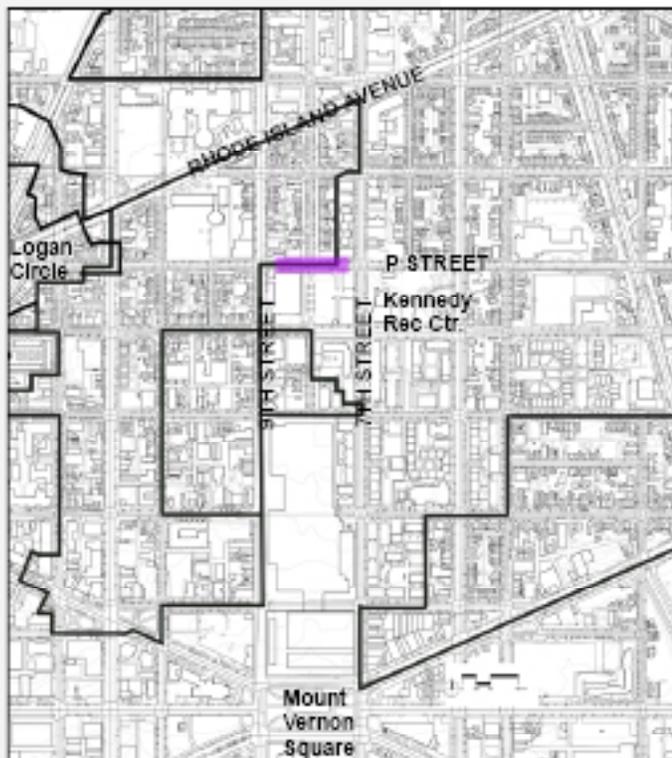


Figure IVj: Location Map

P Street between 7th and 9th Streets is unique to the area in that it incorporates both a low-scale historic residential character on one side of the street, and a potentially large-scale mixed use character on the other side.

Several key issues that characterize P Street are:

- **The north side of P Street** is characterized by a predominance of residential uses with non-residential uses generally located on or near street corners, not in the center of a block. In general, structures are low-rise, two to three stories, and present a human-scale and character.
- **The south side of the street** is currently characterized by the historic O Street Market, Giant food store, and parking lot. In the future, this side of the street will likely be developed as a mixed use project to include substantial residential and commercial activity.
- **Trees lining the street at the curb** are generally located in planting strips or uncovered tree pits that are planted with flowers, grass, or other greenery.
- **P Street** is of particular interest as a major cross-town connector street located between New Jersey Avenue and Logan Circle. The Convention Center Strategic Area Plan recognized its importance by identifying P Street as a "greenway" street that connects a number of the area's open spaces and recreation areas (Figure IVk).

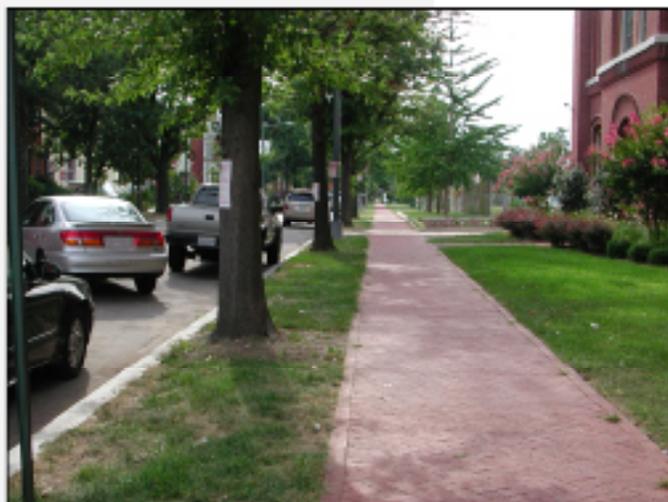


Figure IVk: P Street between 9th & 10th Streets -

IV. Street Character

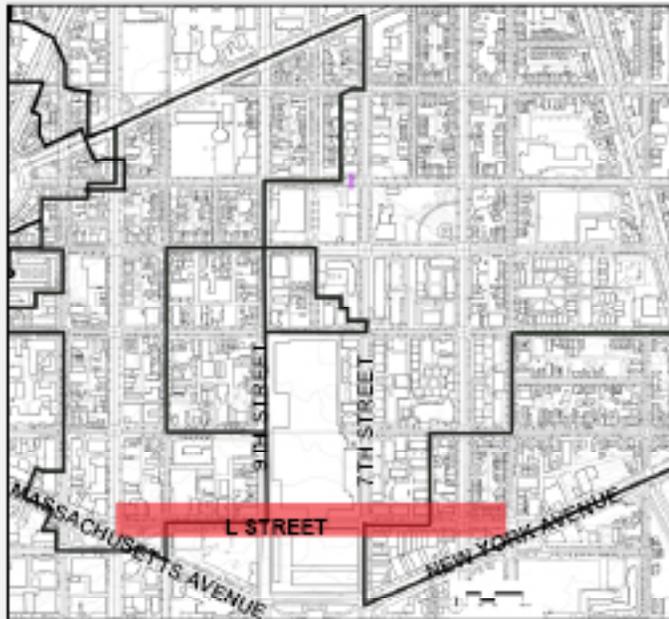


Figure IVi: Location Map



Figure IVm: P Street between 9th & 10th Streets -



Figure IVn: P Street between 9th & 10th Streets -

L Street Connector

L Street at the intersection of New York Avenue and 5th Street is a gateway to the New Convention Center, an introduction to the downtown from the more moderate scale Mt. Vernon Historic District, and a key intersection connecting the Mt. Vernon Square area on the north to the large mixed-use Wax Museum development proposed for the blocks immediately to the south. L Street is also an important pedestrian passageway from one side of the Convention Center to the other. However, few visual elements signal its importance to either vehicles or pedestrians.

Several key issues that characterize L Street are:

- **L Street at the intersection of New York Avenue** is characterized by an unsightly desolate tangle of concrete islands directing traffic through the intersection, long painted step ladder crosswalks on New York Avenue, no pedestrian refuge areas, and high traffic intensities.
- **Approaching L Street from New York Avenue**, there are no visual or directional signage elements indicating access to the convention center, the Mt. Vernon Historic, or streetscape elements that would connect the north side of New York Avenue at L Street to the new Wax Museum development proposed for the blocks located immediately to the south of New York Avenue.
- **L Street between New York Avenue and the New Convention Center** is characterized by old warehouse and industrial buildings with wide broken sidewalks and inconsistent tree plantings on the south side of the street, and low-rise apartment buildings set back from a narrow sidewalk by a wide planting strip and lined with a consistent row of Bradford Pear trees on the north side of the street (Figure IVm).
- **A visitor's introduction to the New Convention Center**, looking westward along L Street from New York Avenue, is characterized by a dark underpass primarily used for bus passenger pick-up and drop-off. The underpass is bridged and flanked by very tall pale blank walls that neither create an effective entranceway image for vehicle passengers nor provide a pleasant pedestrian-scale image for people walking through the center. (Figure IVn)

IV. Street Character

Rhode Island Avenue "Boulevard"

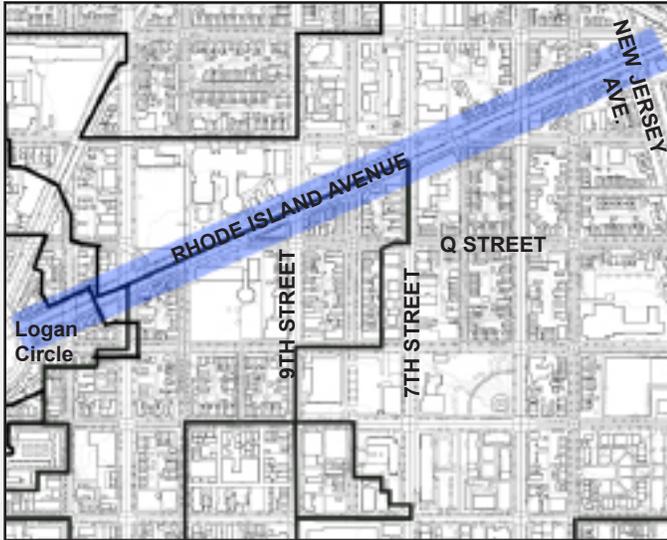


Figure IVo: Location Map

Rhode Island Avenue is a major access route (US Route 1) that has historically connected the District of Columbia with the University of Maryland and the City of Baltimore. In the section extending from 4th Street NE to Logan Circle NW, the avenue takes on the characteristics of a grand boulevard which, as defined by L'Enfant's city plan, is a "broad street with a central median separating the opposite flows of traffic." The boulevard concept is further reinforced by the far reaching vistas that extend from east of North Capital Street to the Potomac River and the skyscrapers of Rosslyn in northern Virginia.

Recent improvements to curbs and gutters, median plantings, and general maintenance have helped to reinforce the boulevard image in the section of Rhode Island Avenue located mostly to the east of Florida Avenue. However, the section between Florida Avenue and Logan Circle, the section that creates a gateway to the Shaw and Mt. Vernon historic districts and to the 7th and 9th Street corridor, requires a variety of improvements.

Several key issues that characterize Rhode Island Avenue are:

- **The large trees that line the boulevard**, and create a mantle of green that frames views to the downtown, are fairly old. (Figure IVp). In many places tree roots have undermined sidewalks lifting the concrete paving six to twelve inches above their original height. This creates a very hazardous pedestrian right-of-way. Unfortunately, cutting the roots in order to level the sidewalk, as implemented in several locations, will ultimately shorten the tree's life.
- **Inconsistent boulevard image from Florida Avenue to Logan Circle** created by different crosswalk patterns, large scale cobra head street lights, a sporadic mix of median plants and trees, and the general lack of a coherent visual image.
- **Corner commercial and retail uses add to the visual clutter.** Although highly desirable, corner commercial and retail uses surrounded by chain link fences, lined wide broken sidewalks and neglected planting areas, and and characterized by vacant looking building fronts greatly detract from the Avenue's image (Figure IVq).



Figure IVp: Tree Lined Streets with Planted Median



Figure IVq: Rhode Island Ave and 7th Street - Visual Clutter

IV. Street Character

Historic Alleyways--Blagden Alley and Naylor Court

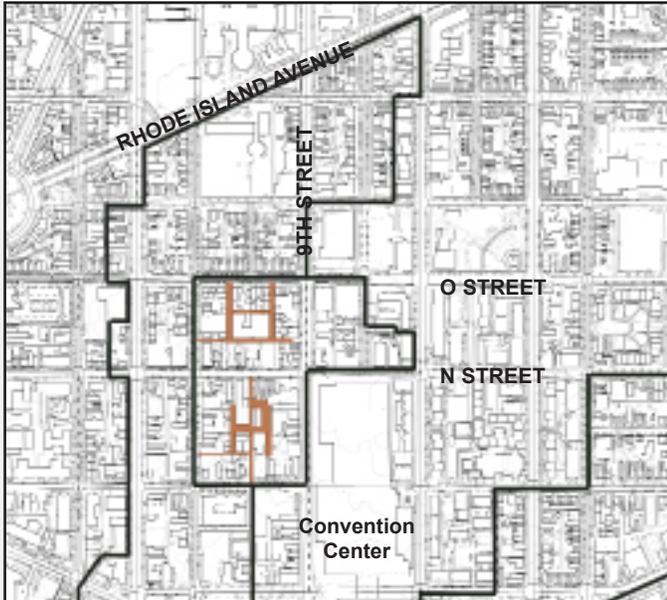


Figure IVr: **Location Map**



Figure IVs: **Newly Paved Alleys**



Figure IVt: **Cobrahead Lighting**

Blagden Alley and Naylor Court are among the few alleyways in the city that are actively occupied with a mix residential and retail uses, and that demonstrate the historic role alleys have played in the economic and social life of the city.

Historically alleyway properties were developed as stables and carriage houses. As the population grew many of the initial commercial uses were succeeded by residential uses often inhabited by household workers and the working poor.

Several key issues that characterize Blagden Alley and Naylor Court are:

- **The historic mix of uses will likely change as the attractiveness of the alleyways continue to grow.** Traditional small retail establishments such as auto oriented businesses currently exist in a sometimes uneasy functional relationship with new businesses and newly renovated residences.
- **DDOT's recently completed installation of brick paving** throughout the Blagden Alley and Naylor Court area creates a significant unifying quality that helps to visually tie the area together (Figure IVs).
- **The use of cobra head street lights in alleyways** creates a contemporary look and lighting scale that detracts from the unique historic character of the alleyways. Although required by DDOT, several options to soften the lighting image, while maintaining security, are needed (Figure IVt).
- **Improvements and decorative detail added to alleyway entrances and the two to four story tall brick buildings** that shape the alleyways help to enhance the historic character and quirkiness of the alleyways.

V. Recommendations

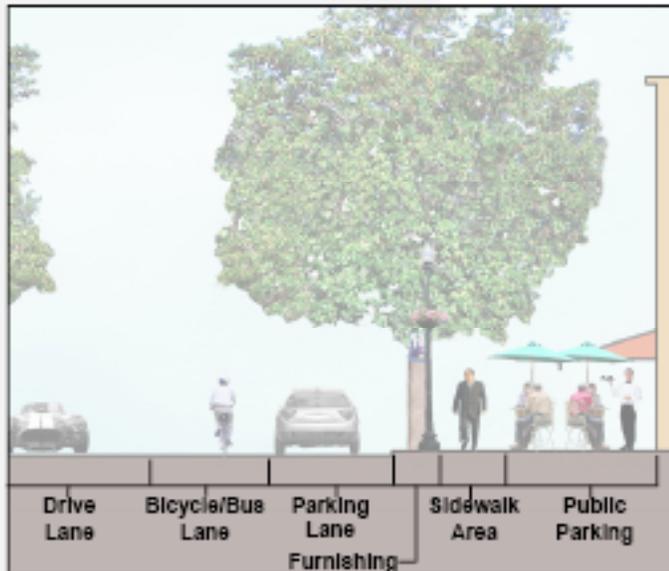


Figure Vb: Street Zones

This section of the Guidelines is intended to provide the DC Department of Transportation (DDOT) and local property owners with design ideas for improving the quality and character of the streets that connect the area together and link it to the city and the region.

The five street types addressed below are:

- Primary Connectors--7th, 9th, and O Streets
- P Street between 7th and 9th Streets
- L Street Connector
- Rhode Island Avenue "Boulevard"
- Historic Alleyways

The recommendations outlined below are intended to produce a coherent visual image for the 7th and 9th Street corridors while also allowing for certain design variations that fit the character of each street type.

In general, recommendations include:

- The consistent use of sidewalk paving materials
- The consistent use of Washington Globe street lights
- Similar crosswalk design, tree box design, tree and planting material selections
- Similar street furniture design

The design concepts that are presented after each set of recommendations are intended to spark additional creative thinking about how each street's character may be further developed and enlivened.

Definitions

Sidewalks and streets are generally divided into different functional zones. Sidewalks, the area reserved for pedestrians located between the street curb and building or property line, are generally defined by the following:

- **Sidewalk Area** -- the public right-of-way for walking should be a minimum of 4 feet wide and allow for the unobstructed flow of pedestrian traffic. The sidewalk corner that allows people to wait in an unobstructed area before crossing a street is considered part of the pedestrian zone.
- **Furnishing Area**--the strip of sidewalk adjacent to the curb may be as narrow as one foot or as wide as a five feet. The Curb Zone contains the elements that support pedestrian use of the sidewalk such as street trees for shade, parking meters, bicycle racks, street signs, street light poles, plantings, mail boxes, street furniture and benches, vendors and vendor carts, newspaper boxes, traffic signal and utility boxes, among other elements.

V.
Recommendations
 and Concepts



Figure Vc: **Crosswalks Visually Standing Out**



Figure Vd: **Granite Curb and Brick Gutters**



Figure Ve: **Bike Lane**

- **Public Parking**--in general, this zone will overlay a portion of the Pedestrian Zone or the Curb Zone to allow for a sidewalk amenity at a specific location. Amenities may include elements such as an outdoor café, merchandise display area, advertisement, small public seating areas, or a work or art. Generally amenity zones are located in front of a specific property, although they may also include small sitting areas located adjacent to the sidewalk in an area created by a building setback.

Depending on sidewalk width it may not be possible to incorporate all three zones in a sidewalk design.

Streets, the area reserved for vehicles located between the street curbs, are generally defined by the following zones:

- **Crosswalks and intersection boxes** designed to connect sidewalks at street crossings and to visually define the intersection between corners.
- **Curbs and gutters** designed to capture water runoff and to define the edge of the sidewalk and separate vehicular and pedestrian traffic (Figure Vd).
- **Parking lanes** designed as either parallel or diagonal configurations to allow vehicles to park on the street adjacent to the street curb.
- **Drive lanes** designed to allow the unobstructed flow of vehicular traffic
- **Turn lanes and merge lanes** designed to allow vehicles to either exit or enter drive lanes
- **Shared Bicycle/Bus lanes** designed to allow buses and bicycle traffic to move quickly through other vehicular traffic (Figure Ve).
- **Median strips** designed to separate wide multi-lane streets with a safety barrier between oncoming lanes of traffic. Medians are often landscaped to create a boulevard effect.
- **Safe pedestrian zones** designed at the intersection of median strips and crosswalks to allow pedestrians to shorten their walk across wide streets and to rest in a safe area in the middle of the street.

Primary Connectors --
7th, 9th and O Streets



Figure Vf: Streetscape Character Plan



Figure Vg: Streetscape Character Plan

Objectives

- Create a quality pedestrian environment that a) results in a coherent visual image for the 7th and 9th Street corridors from Mt. Vernon Square to Rhode Island Avenue
- Attract people from downtown to area businesses
- Link the downtown, through the area to the Uptown Entertainment District
- Provide a good pedestrian environment for the Heritage Trail and sidewalks that connect the area's historic districts
- Enhance the important role of the O Street Market as a central "place"
- Preserve the historic character of the area

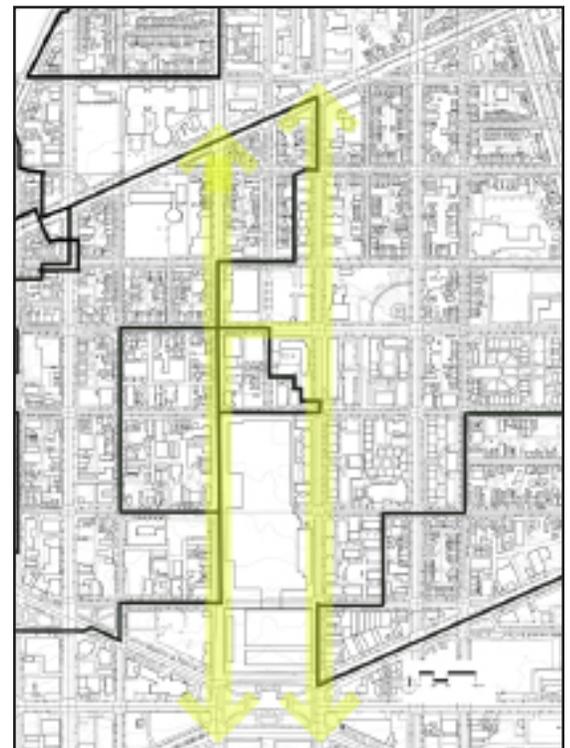
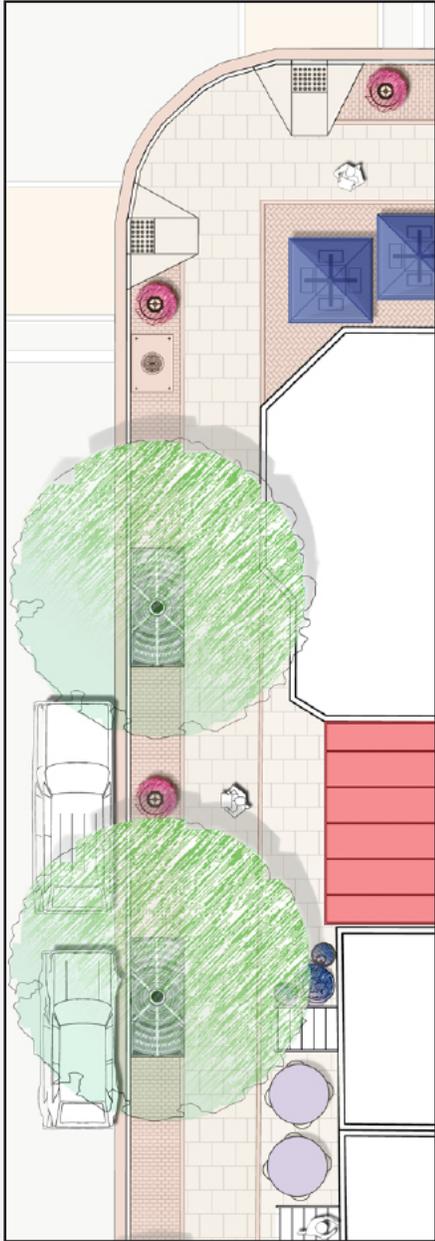


Figure Vh: Location map

V. Primary Connectors
 -- 7th, 9th and O
 Streets



FURNISH-
ING AREA

PEDESTRIAN
ZONE

PUBLIC
PARKING

Figure Vi: *Enlarged Streetscape Plan*

Recommendations

- **Pedestrian zone**
Concrete unit pavers in an aggregate color should be used to define the pedestrian zone, the sidewalk area located between the building or property line and the curb or curb zone.

In general, the pedestrian zone should be no less than 6 feet wide and provide an unobstructed pathway to allow two people to walk side by side.

- **Furnishing area**
Brick pavers in a running bond pattern should be used to visually define the curb zone located between the pedestrian zone and the curb.

In general, curb zones should be no less than 4 and up to 6 feet wide.

Curb zones should not be incorporated in sidewalks that are less than seven feet wide from building or property line to the street curb.

When street trees are incorporated in the curb zone, curb zone width should be no less than 4 feet to allow for adequate tree box size and drainage. On narrow sidewalks, portions of the pedestrian zone may be reduced to no less than 6 feet wide to accommodate a 4 feet wide tree box.

- **Handicap ramps**
Handicapped ramps that connect the sidewalk corner with the street crosswalk should be made of concrete to match pedestrian zone color.

Ramps should incorporate a detectable warning surface texture that differentiates the ramp from the concrete pavers.

- **Public parking**
Concrete unit pavers in a contrasting but complementary color with the pedestrian zone should be used to visually define amenity zones. Four inch wide brick banding in a running bond pattern should define amenity zone edges.

Amenity zones should be no less than five feet wide and should allow for a minimum of a four feet wide unobstructed pedestrian right-of-way adjacent to all sides of the zone.

Amenity zones may overlay areas in the pedestrian zone without obstructing pedestrian traffic, or overlay areas in the curb zone. Amenity zone paving provided by individual property or business

V. Primary Connectors -- 7th, 9th and O Streets



Figure Vj: **Outdoor Cafe**

owners may be incorporated into the design, especially for aesthetic purposes such as artist inspired amenity zone murals or paving patterns using special paving materials.

- **Street Lighting**

Single Washington Globe street lights should be installed at the street curb throughout 7th, 9th, and O Streets in a similar spacing pattern to the street lighting located around the New Convention Center.

- **Street trees**

The following DDOT approved street trees should be installed:

7th Street

Chinese Elm - *Ulmus parviolia*

Princeton Elm - *Ulmus americana* 'Princeton'

Willow Oak - *Quercus phellos*

9th Street

Red Maple - *Acer rubrum*

Princeton Elm - *Ulmus americana* 'Princeton'

Japanese Zelkova - *Zelkova serrata*

O Street

Red Maple - *Acer rubrum*

Golden Raintree - *Koelreuteria paniculata*

- **Sidewalk furniture**

Standard sidewalk furniture approved by DDOT should be limited to the following items:

Plainwell Landscape Forms benches, painted black with required center arm rest. (*Benches should be located in areas to be determined by the community or at secure locations that are desired by local property or business owners.*)

Traditional trash bins painted black should be located near intersections, two to a block face.

U-shaped bicycle rack, painted black, should be located in appropriate store front or building entranceway locations.

Sidewalk furniture provided by individual property or business owners may be provided in any design appropriate to the intended use in amenity zones--especially when designed for aesthetic purposes such as works of public art such as artist inspired chairs, tables, fencing that defines the amenity area, awnings, or other elements that will enliven the streetscape without obstructing pedestrian zone traffic.

V. Primary Connectors -- 7th, 9th and O Streets

- **Curbs and gutters**
Install standard DDOT granite curbs and brick gutters throughout primary connector streets.
- **Visual buffers**
Surface parking lots and vacant lots facing onto the public sidewalk should be visually buffered from public view. Low fencing, decorative planting, and land berm designs may be considered. Consideration for CPTED issues should be given in the design of buffers.
- **Bicycle paths**
The east side of 7th Street should incorporate a shared bicycle/bus lane consistent with 7th Street below Mount Vernon Square.
- **On street parking**
The amount and location of on street parking should not be altered by the recommendations proposed, however, see Concepts for Further Consideration discussed below.
- **Bus Stops**
Brick pavers in a running bond pattern similar to the curb zone paving pattern should be incorporated into bus stop design to visually associate the bus stop area with the curb zone. Consideration should be given to incorporating display photos and posters of historic neighborhood features in the standard DDOT bus shelter structure design.

Concepts for Further Considerations

- *West side of 9th Street between Mt. Vernon Square and N Street*

Depending on the results of the DDOT vehicular capacity study for 9th Street, several potential concepts for improving the pedestrian zone, which is extremely narrow in most places, are possible.

Option A

Widen the sidewalk to replace the existing parking-lane and create a new parking lane in a drive-lane. This option will retain current levels of on-street parking and reduce the number of drive lanes by one.

Option B

Widen the sidewalk to replace the current parking lane. This option will reduce on-street parking in the area. It will also create significant pedestrian access without limiting vehicular traffic.

Option C

Widen the sidewalk to replace the current parking lane at narrow sidewalk areas only by creating bulb-outs or nubs thereby allowing pedestrians to walk around existing obstructions. This option will reduce, but not totally eliminate, the number of on-street parking spots. It will also make it

V. Primary Connectors -- 7th, 9th and O Streets

impossible to utilize the lane for rush-hour traffic.



Figure VI: **Proposed 9th Street Widening**

Option D

Maintain the existing narrow sidewalk width and create access to businesses from both 9th Street and the alleyway. This option will create ground floor retail entrances on two sides of the buildings lining 9th Street which will also likely increase pedestrian traffic along the alleyway.

O Street and the O Street Market Site located between 7th and 9th Street

In order to highlight the importance of O Street, consider implementing special creative artist inspired designs for the intersection box areas and the corner areas at 7th and 9th Streets.

To increase and improve pedestrian access consider the creation of a median strip on O Street from 7th Street to 9th Street to include opportunities for pedestrian safe zones, locations for public art, and decorative tree and planting selections.

To take advantage of the south facing side of the O Street Market site, consider the creation of a significantly wide sidewalk that will be a platform for outdoor cafes, sidewalk activities, and display areas. In order to facilitate pedestrian traffic, consider locating a traffic signal and a special crosswalk design at the intersection of 8th and O Streets.

Consider implementing public art installations, or special tree and planting designs, on all four sides of the O Street Market site, that are appropriate to the scale of buildings and activities on the opposite side of the street. For example, a decorative tree species planted at all four corners of the site may add to its visual identity.

Consider implementing public art works that will mitigate the affects of blank walls or other undesirable visual elements, especially ones facing onto the important streets that surround the O Street Market site.

V. P Street between 7th and 9th Streets

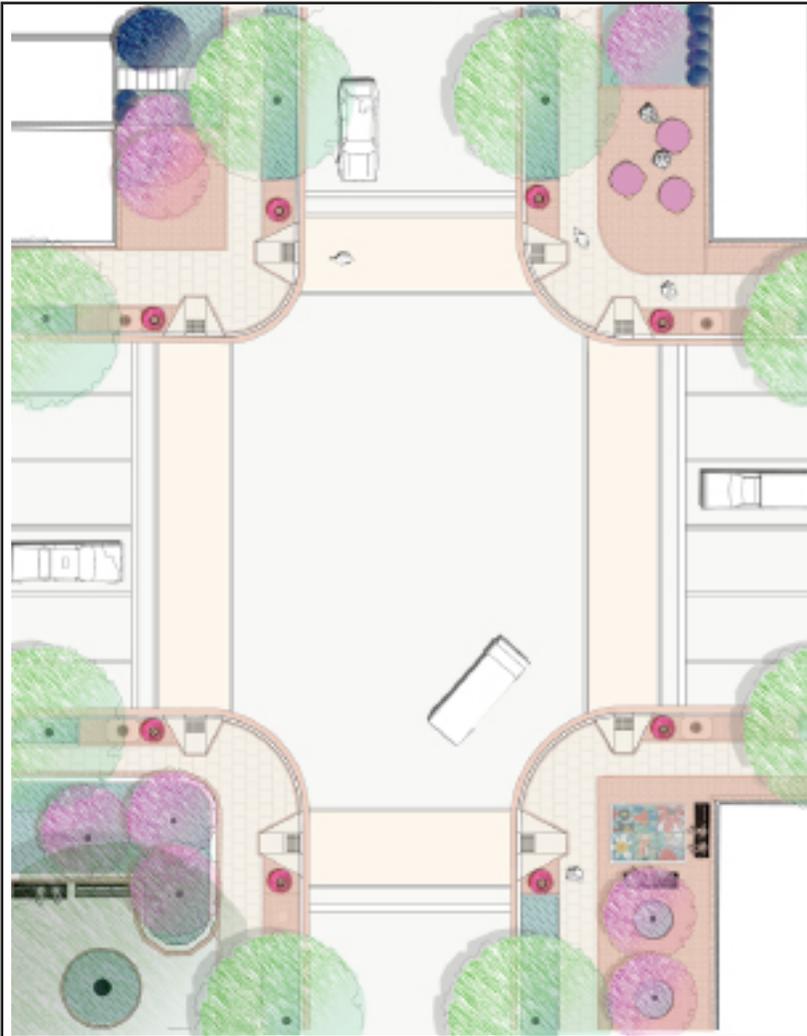


Figure Vm: Streetscape Plan



Figure Vn: Streetscape Section

Objectives

- Maintain and build on the traditional and historic character of the area on both sides of the street
- Protect the residential character of the sidewalks and front yard areas on the north side of the street

Recommendations

- **Pedestrian zone**
Use concrete unit pavers in an aggregate color to define the pedestrian zone, the sidewalk area located between the building or property line and the curb or curb zone. In general, the pedestrian zone should be no less than four feet wide and provide an unobstructed pathway to allow two people to walk side by side. In all cases, brick banding in



Figure Vo: Location Map

V. P Street between 7th
and 9th Streets



Figure Vp: **Enlarged Streetscape Plan**



Figure Vq: **Front Yard Treatment and Ornamental Railing**

four inch wide running band pattern should define the edges of the pedestrian zone.

Great care must be given to protecting and maintaining the historic front yard areas of residential properties, especially fences defining the front yard and walkways or steps providing access to the residential property.

- **Curb zone**

P between 7th and 9th Streets should incorporate planting strips and brick pavers in an alternating pattern as a curb curb zone. This arrangement will to help reinforce the residential character of the street as well as provide access to the sidewalk from parked cars.

In general, curb zones should be no less than 4 and up to 5 feet wide. Curb zones should not be incorporated in sidewalks that are less than seven feet wide from building or property line to the street curb.

When street trees are incorporated in the curb zone, curb zone width should be no less than 4 feet to allow for adequate tree box size and drainage. On narrow sidewalks, portions of the pedestrian zone may be reduced to no less than 4 feet wide to accomodate a 4 feet wide tree planting area within the curb zone.

- **Handicap ramps**

Handicapped ramps that connect the sidewalk corner with the street crosswalk should be made of concrete to match pedestrian zone color. Ramps should incorporate a detectable warning surface texture that differentiates the ramp from the concrete pavers.

- **Amenity zones**

Although there are likely to be few amenity zones on P Street, the following recommendations should apply:

Amenity zones should be no less than five feet wide and should allow for a minimum of a four feet wide unobstructed pedestrian right-of-way adjacent to all sides of the zone.

Sidewalk furniture provided by individual property or business owners may be provided in any design appropriate to the intended use in amenity zones--especially when designed for aesthetic purposes such as works of public art such as artist inspired chairs, tables, fencing that defines the amenity area, awnings, or other elements that will enliven the streetscape without obstructing pedestrian zone traffic.

V. P Street between 7th and 9th Streets

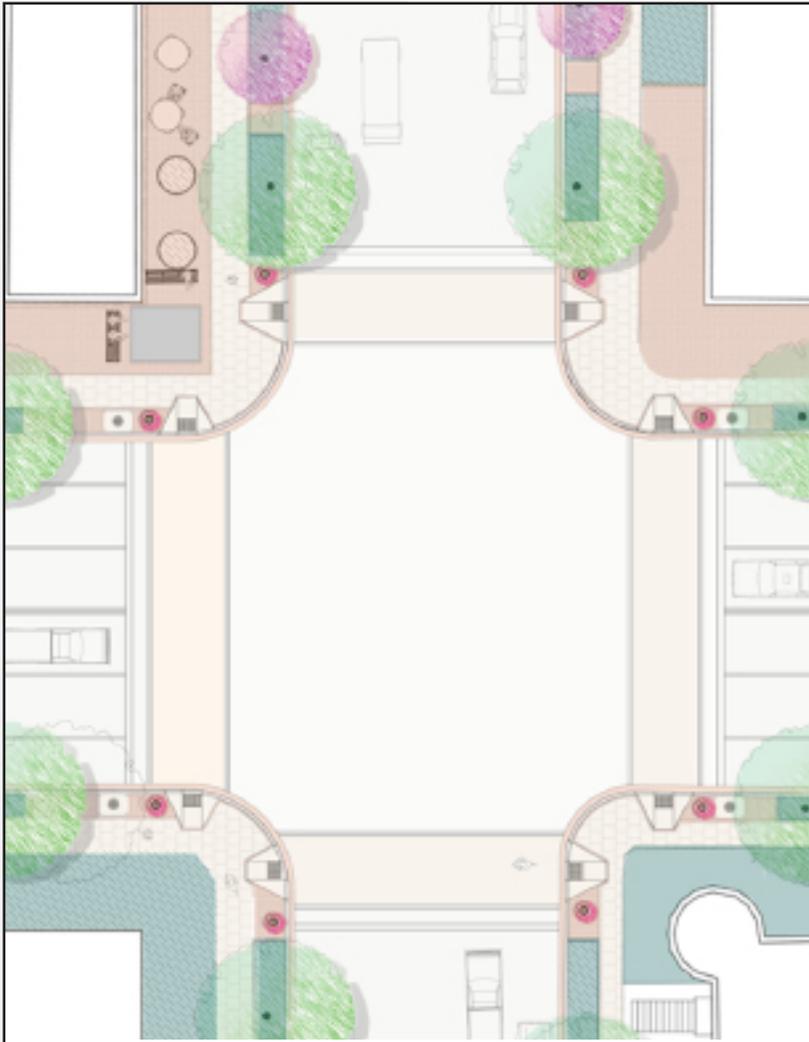
- **Street Lighting**
Single Washington Globe street lights should be installed at the street curb on all secondary streets. Teardrop street lights should be installed on the corners of all intersections.
- **Street trees**
The following DDOT approved street trees should be installed:

P Street
Willow Oak - *Quercus phellos*;
Sweetgum - *Liquidambar styracifua*
- **Tree Planting Boxes**
Planting beds shall be made flush with the sidewalk.
Planting bed edges shall be defined by a four inch wide brick banding in a running bond pattern.
- **Street furniture**
In general, no public street furniture is recommended for secondary streets other than furniture that may be provided in amenity zones.
- **Curbs and gutters**
Curbs should be the standard DDOT granite curbs with brick gutters.
- **Visual buffers**
Surface parking lots and vacant lots facing onto the public sidewalk should be visually buffered from public view. Low fencing, decorative planting, and land berm designs may be considered. Consideration for CPTED issue should be given in the design of buffers.
- **Bicycle Paths**
No bicycle lanes are anticipated.
- **On Street Parking**
The amount and location of on street parking should not be altered with the general recommendations proposed.

Concepts for Further Considerations

- **Tree box and curb zone planting beds**
The most significant elements that will help to define the visual character of P street, other than the sidewalk pattern outlined above, are planting strips located along the curb zone or at the base of street trees that line each side of the street.
- Visual interest would be added to the streetscape if the planting strips on both sides of the street were to be planted with perennials and low ground covers that are specific to that street. This approach would give added visual detail and uniformity to this section of P Street.

V. L Street Connector



Vr: Streetscape plan



Vs: Streetscape section



Vt: Location map

Objectives

- Create a quality pedestrian environment along L street as a gateway to the New Convention Center and downtown
- Enhance the visual image of the New Convention Center at the pedestrian level

Recommendations

- **Pedestrian zone**
Poured in place concrete in an aggregate color should be used to define the pedestrian zone, the sidewalk area located between the building or property line and the curb or curb zone. In general, the pedestrian zone should be no less than 6 feet wide and provide an unobstructed pathway to allow two people to walk side by side.

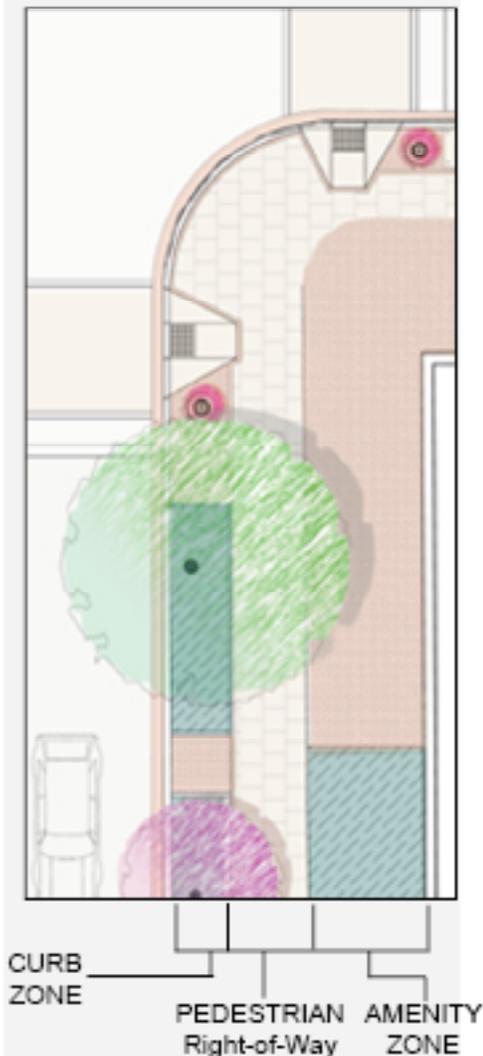


Figure Vw: Enlarged Streetscape Plan

Brick banding in a four inch wide running bond pattern should be used to define the edges of the pedestrian zone.

- **Curb zone**

Brick pavers in a running bond pattern should be used to visually define the curb zone located between the pedestrian zone and the curb.

In general, curb zones should be no less than three and up to five feet wide.

Curb zones should not be incorporated in sidewalks that are less than seven feet wide from building or property line to the street curb.

When street trees are incorporated in the curb zone, curb zone width should be no less than 4 feet to allow for adequate tree box size and drainage. On narrow sidewalks, portions of the pedestrian zone may be reduced to no less than three feet wide to accommodate a five feet wide tree tree box.

- **Handicap ramps**

Handicapped ramps that connect the sidewalk corner with the street crosswalk should be made of concrete to match pedestrian zone color.

Ramps should incorporate a detectable warning surface texture that differentiates the ramp from the concrete pavers.

- **Amenity zones**

Concrete unit pavers in a contrasting but complementary color with the pedestrian zone should be used to visually define amenity zones. Four inch wide brick banding in a running bond pattern should define amenity zone edges.

Amenity zones should be no less than 5 feet wide and should allow for a minimum of a 6 feet wide unobstructed pedestrian right-of-way adjacent to all sides of the zone.

Amenity zones may overlay areas in the pedestrian zone that are adjacent to buildings or in the curb zone.

Sidewalk furniture provided by individual property or business owners may be provided in any design appropriate to the intended use in amenity zones--especially when designed for aesthetic purposes such as works of public art such as artist inspired chairs, tables, fencing that defines the amenity area, awnings, or other elements that will enliven the streetscape without obstructing pedestrian zone traffic.

- **Street Lighting**

Single Washington Globe street lights should be installed at the street curb on both sides of L Street in a similar spacing pattern to the street lighting located around the New Convention Center. Banner fixtures should be incorporated on all single and double Washington Globe light poles.

- **Street trees and planting beds**

The following DDOT approved street trees should be installed:

L Street

London Planetree - *Platanus acerifolia*

Littleleaf Linden - *tilia cordata*

Decorative flowering planter boxes at sidewalk level may be designed to fit between the existing Bradford Pear trees in a manner that does not interrupt street parking functions.

- **Tree planting boxes**

Tree boxes may be designed to contain plant and flower materials in a manner similar to the planting beds described above.

- **Sidewalk furniture**

Standard sidewalk furniture approved by DDOT should be limited to the following items:

Plainwell Landscape Forms benches, painted black with required center arm rest. Benches should be located in areas to be determined by the community or at secure locations that are desired by local property or business owners.

Tradition trash bins with domed lid, painted black should be located near intersections, two to a block face.

U-shaped bicycle rack, painted black, should be located in appropriate store front or building entranceway locations according to DDOT design manual.

Sidewalk furniture provided by individual property or business owners may be incorporated into the design of amenity zones, especially for aesthetic purposes such as works of public art. In these instances, street furniture may include artist produced chairs, tables, fencing to define outdoor cafe areas, awnings, or other elements that will enliven the streetscape without obstructing pedestrian zone traffic.

- **Curbs and gutters**

Install standard DDOT granite curbs and brick gutters throughout primary connector streets.

- **Visual buffers**

Surface parking lots and vacant lots facing onto the public sidewalk should be visually buffered from public view. Low fencing, decorative planting, and land berm designs may be considered. Consideration for CPTED issues should be given in the design of buffers.

- **Bicycle paths**
Dedicated bicycle paths are not anticipated for L Street.
- **On street parking**
The amount and location of on street parking should not be altered by the recommendations proposed, however, see Concepts for Further Consideration discussed below

Concepts for Further Considerations

- Develop a gateway design for the L Street, 5th Street, and New York Avenue intersection to create a sense of entry to the New Convention Center and the downtown. Utilize the concrete traffic islands or areas adjacent to the intersection for public art or other landscape and public information elements to create a gateway image.
- Redesign the intersection and median with crosswalks that visually connect the north and south sides of New York Avenue thereby linking the Mt. Vernon Historic District and the surrounding area. Utilize double Washington Globe street lights throughout the intersection to provide a pedestrian scale that will mitigate the industrial, expressway, scale of the cobra head street lights that currently dominate New York Avenue.
- Utilize decorative bollards on street corners at 7th and 9th Streets--locate the bollards to either side of handicap ramps at crosswalks, to help direct pedestrian traffic, and to highlight the importance of the two intersections that lead to and from the New Convention Center.
- Enhance the visual image of L Street as it moves through the Convention Center between 7th and 9th Streets. Consider the use of artist inspired paving patterns and materials, architectural lighting, and the use of public art. Also consider the use of raised planting boxes large enough for substantial decorative tree species and ground covers

Utilize banners, fabric art, or other durable material throughout the bus loading area to create a sense of motion and kinetic decoration

V. Rhode Island Avenue "Boulevard"



Figure Vy: Streetscape Plan



Figure Vz: Streetscape Section

Objectives:

- Improve the visual quality of Rhode Island Avenue as a gateway corridor—a grand boulevard—to the Shaw Historic District and the downtown
- Create a unifying design that links visual quality of Rhode Island Avenue together
- Install pedestrian friendly sidewalks
- Protect and maintain the large mature trees that line Rhode Island Avenue

Recommendations

- **Pedestrian zone**
Poured in place concrete in an aggregate color should be used to define the pedestrian zone, the sidewalk area located between the building or property line and the curb or curb zone.

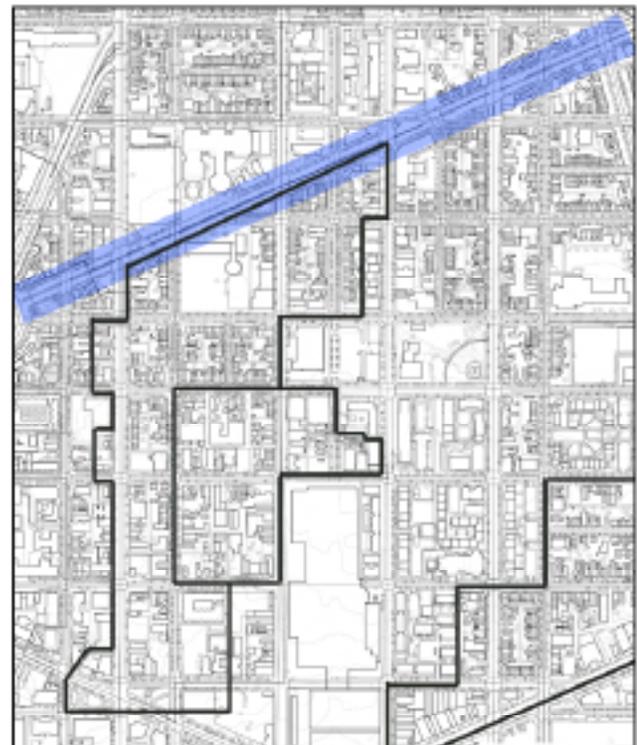


Figure Vaa: Location Map

V. Rhode Island
Avenue
"Boulevard"

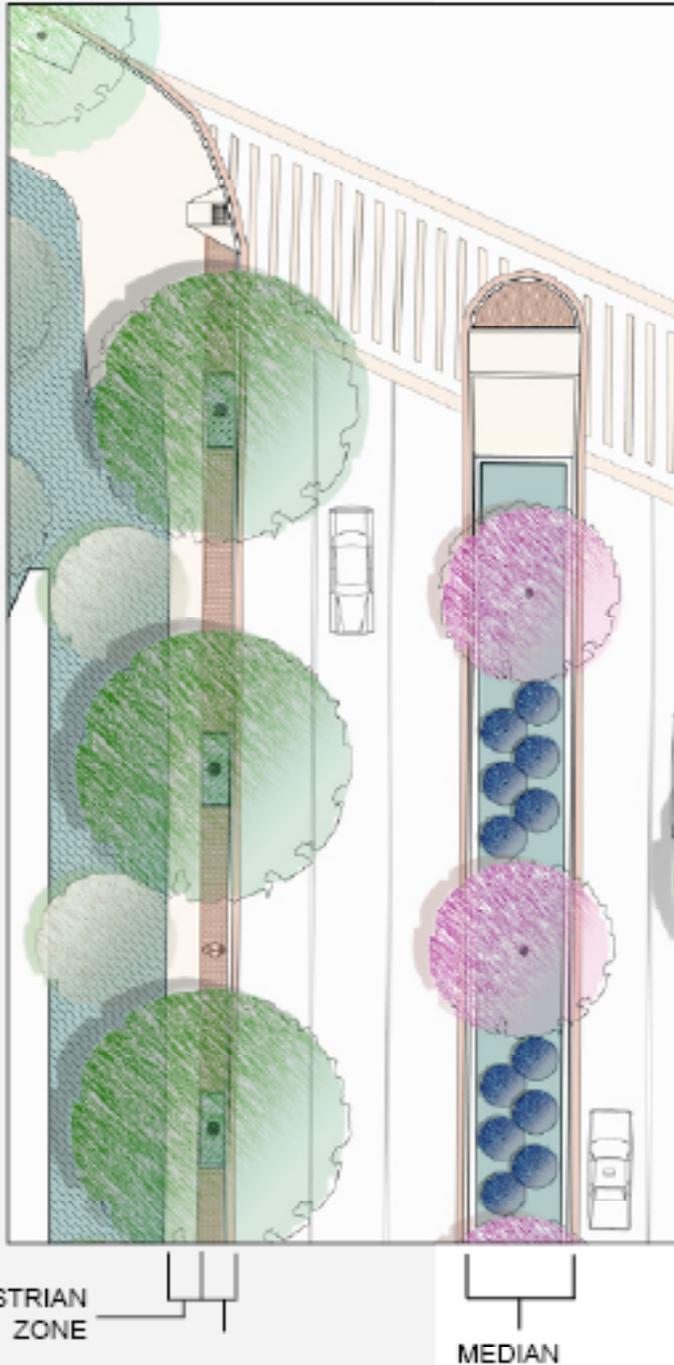


Figure Vab: Enlarged Streetscape Plan

In general, the pedestrian zone should be no less than 4 feet wide and provide an unobstructed pathway to allow two people to walk side by side. However, at a number of locations along Rhode Island Avenue, maintaining a four feet wide pedestrian zone may be impossible due to the narrow distance between the curb and property lines. In those instances, the historic property line must be maintained.

Due to the narrow sidewalk width and the heaving affect established mature trees have had on the sidewalk, brick banding is not required along Rhode Island Avenue sidewalks.

Great care must be given to protecting and maintaining the historic front yard areas of residential properties, especially fences defining the front yard and walkways or steps providing access to the residential property.

- **Handicap ramps**
Handicapped ramps that connect the sidewalk corner with the street crosswalk should be made of concrete to match pedestrian zone color.

Ramps should incorporate a detectable warning surface texture that differentiates the ramp from the concrete pavers.

- **Amenity zones**
Of particular interest along Rhode Island Avenue are the small neighborhood stores and retail establishments located at several street corners between Florida Avenue and Logan Circle.

Concrete unit pavers in a contrasting but complementary color to the pedestrian zone should be used to visually define amenity zones. Four inch wide brick banding in a running bond pattern should define amenity zone edges.

Sidewalk furniture provided by individual property or business owners may be provided in any design appropriate to the intended use in amenity zones--especially when designed for aesthetic purposes such as works of public art, artist inspired chairs, tables, fencing that defines the amenity area, awnings, or other elements that will enliven the streetscape without obstructing pedestrian zone traffic.

V. Rhode Island Avenue "Boulevard"

- **Street Lighting**
Double Washington Globe street lights should be installed on both sides of Rhode Island Avenue.

Banner fixtures should be incorporated on all single and double Washington Globe light poles lining the 9th Street portion of the secondary streets located from P Street to Rhode Island Avenue only.
- **Street trees**
The following DDOT approved street trees should be located along Rhode Island Avenue: -Willow Oak - Quercus phellos (Sidewalk) Okame Cherry - Prunus 'Okame' (Median) New trees should be installed with root barriers to prevent future sidewalk damage.
- **Street furniture**
In general, no public street furniture is recommended for these streets other than furniture that may be provided in amenity zones or located in public reserves such as Mother Cooper Park and the Carter G. Woodson Park.
- **Curbs and gutters**
Curbs should be the standard DDOT granite curbs with brick gutters.
- **Visual buffers**
Surface parking lots and vacant lots facing onto the public sidewalk should be visually buffered from public view. Low fencing, decorative planting, and land berm designs may be considered. Chain link fencing should be strongly discouraged. Consideration for CPTED principles should be given in the design of buffers.
- **Bicycle Paths**
No bicycle lanes are anticipated.
- **On Street Parking**
The amount and location of on street parking should not be altered with the general recommendations proposed. The need for Rush Hour Restrictions should be examined in the future.

Concept for Further Considerations

- **Gateway at New Jersey Avenue**
Develop a gateway design for the Rhode Island Avenue at New Jersey Avenue to create a sense of entry to the historic Shaw and Mt. Vernon Square areas. Utilize the triangle shaped reserve between New Jersey and Florida Avenues, or the median strip immediately to the west of New Jersey Avenue for public art or other landscape and public information elements to create a gateway image. Install planting buffers around the gas stations and open spaces that currently define the New Jersey and Rhode Island Avenue intersection.
- **Enhance the importance of the median**
Enhance the design of the median strip from New Jersey Avenue to

V. Rhode Island Avenue “Boulevard”

Logan Circle to create a unified “boulevard” image for the corridor. Due to the existing large trees that already visually define the two sides of the boulevard, the median becomes the primary element for enhancing the avenue’s appearance.

Consider creating 32 inch high raised planter beds along the median strip with decorative concrete sidewalls per DDOT standards. This treatment will help to define the median strip, improve public safety, Create a clearly defined pedestrian safety zone at crosswalks, and create a clear visual image that will be unique to the boulevard. Install unique ground covers and natural grass plant materials planted in a well designed and unique fashion to add to the boulevard image. Consider installing Washington Globe lights along the middle of the median to enhance street lighting and replace existing cobra head lights. Median lighting would create an exceptional boulevard image.

- **Correct sidewalk buckling due to root growth without cutting the tree root system.**

In order to mitigate sidewalk buckling at the base of mature trees lining the boulevard, replace the buckled sidewalk sections with a smooth curved section that bridges the root system without damage. While this is not the best solution, the question becomes one of prolonging the life of mature trees that define the visual character of the boulevard, or dramatically reducing tree life in favor of a flat sidewalk. Although the suggested solution is not optimal, the proposed smooth surface will improve pedestrian access along the sidewalk while protecting the remaining life of the mature trees.

- **Create art sites at corner stores and public parks.**

Utilize public open spaces and corner retail stores as public art sites to create an "Art Boulevard". The street corners between Florida Avenue and Logan Circle that present opportunities for installing public art are:

- Florida and Rhode Island Avenue reserve on the north side
- The retail store on the South/East corner of 5th and Rhode Island Avenue
- Mother Cooper Park at 6th and Rhode Island Avenue
- The wide sidewalk on the north side of 7th and Rhode Island Avenue
- The W. T. Daniel Library site at the intersection of 7th and Rhode Island Avenue
- The retail store on the South/West corner of 8th and Rhode Island Avenue
- Carter G. Woodson Park at 9th and Rhode Island Avenue
- The Shaw Junior High School site at 10th and Rhode Island Avenue
- The triangular traffic island at 11th and Rhode Island Avenue
- The retail establishments located on the north and south corners of Rhode Island Avenue and 11th Street
- The park located on the south side of Rhode Island Avenue between 12th Street and Logan Circle

Blagden Alley and Naylor Court

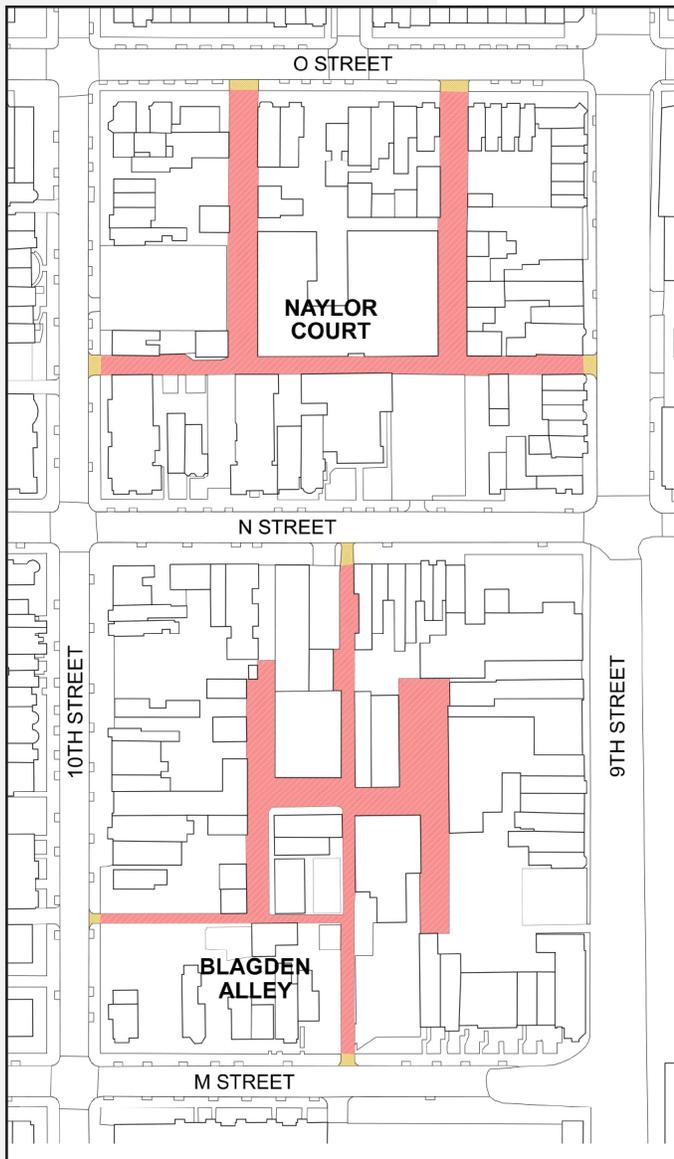


Figure Vab: Alley Plan

Objectives

- Build on the recent efforts by DDOT to improve Blagden Alley and Naylor Court as unique historic environments that serve a mix of residential, commercial, retail, and institutional uses.

The recently installed brick paving throughout Blagden Alley and Naylor Court by DDOT has dramatically enhanced their image and improved their use by both residential and commercial properties served by the alleyways.
- Enhance the human-scale and historic character of Blagden Alley and Naylor Court Historic District that presents a unique urban setting for the people who live and work in this area of the city.

Recommendations

- **Maintain the newly installed brick paving**
Maintain the new brick paving in running bond pattern throughout all alleyways. Any repairs made to the new paving should be matched to, and patched with, the same brick and paving pattern.



Figure Vac: Location Map

Blagden Alley and Naylor Court



Figure Vad: *Alley Entrance Apron*



Figure Vae: *Pole Mounted Hanging Flower Basket*

- **Lighting**
Cobra head street lights, which are the DDOT standard for alleyways, should be painted black to create a unified image to the modern looking gray metal fixtures.

Banner hardware should be incorporated on all single and double Washington Globe light poles.

Consideration should be given to include hanging flower basket hardware on light poles to provide the opportunity for local citizens, organizations or institutions to install flower baskets and maintain flowers.

Concept for Further Considerations

- **Alleyway entranceways to buildings**
To mitigate the large-scale intensity of the cobrahead streetlights consider the use of decorative outdoor lighting at building entranceways. Located head-high, entranceway lighting will provide additional security for pedestrians and create a more intimate night time image.
- **Gateway treatments at alley entrances**
Consider creating gateway elements at entranceways to Blagden Alley and Naylor Court, from major city streets, that will clearly define the boundaries, establish a clear sense of territory and defensible space, and provide opportunities for entranceway signage.
- **Visual enhancements**
Consider the addition of flower boxes, banners, decorative wall murals, hanging signs, and the use of highlight colors to emphasize building entrances that will add visual interest and improve access to alley businesses and residences.

VI. Appendix

Lighting



*Vlc: Single Washington
Globe Light*



*Vld: Double
Washington Globe*



*Vle: Cobrahead
Streetlight*

	Single Globe	Double Globe	Cobrahead
Primary		●	
Secondary	●	●	
Rhode Island Ave.		●	
L Street		●	
Alleys			●

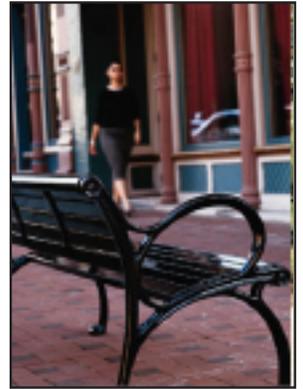
Street Furniture



Vlf: Bike Rack



*Vlg: Plainwell Trash
Receptacle*



Vlh: Plainwell Bench

	Bike Rack	Trash Receptacle	Bench
Primary	●	●	●
Secondary		●	
Rhode Island Ave.	●	●	●
L Street	●	●	
Alleys			

Appendix

Paving



Vii: Concrete Pavers



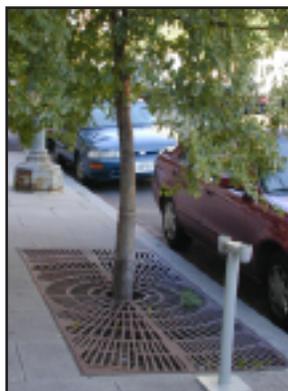
Vlj: Concrete Unit Pavers



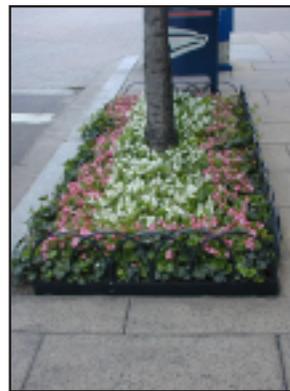
Vlk: Amenity Zone Possibilities

	Curb Zone	Pedestrian Zone	Amenity Zone
7th Street	Brick	Unit Paver	approved paving
9th Street	Brick	Unit Paver	approved paving
L Street	Brick	Unit Paver	approved paving
O Street	Brick	Unit Paver	approved paving
P Street	Brick & Planting	Unit Paver	front yard/paving
Rhode Island Ave	N/A	Unit Paver	front yard/paving

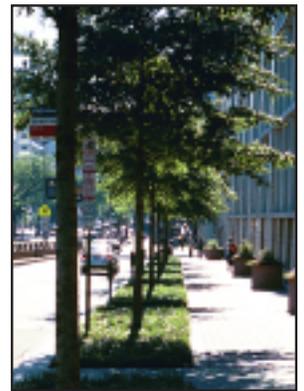
Treepits



Vll: Tree grate



Vlm: Planted treepit



Vln: Continuous Treepit

	Tree Grates	Open Treepits	Continuous Pits
Primary	●		
Secondary		●	● Curb zone
Rhode Island Ave.	●	●	● Median
L Street	●		● Amenity Zone
Alleys			

Appendix

Canopy Trees



Vlo: Acer rubrum 'October Glory' Red



Vlp: Koelreuteria paniculata Golden



Vlq: Liquidambar styraciflua Sweet Gum



Vlr: Platanus acerifolia London



Vls: Quercus phellos Willow Oak



Vlt: Tilia cordata Littleleaf Linden



Vlu: Ulmus americana 'Princeton' Princeton



Vlv: Ulmus parvifolia Chinese Elm



Vlw: Zelkova serrata Japanese Zelkova

	AR	KP	LS	PA	QP	TC	UA	UP	ZS
7th Street					●		●	●	
9th Street	●							●	●
O Street	●	●							
L Street				●		●			
P Street			●		●				
Rhode Island Ave.					●				

Appendix

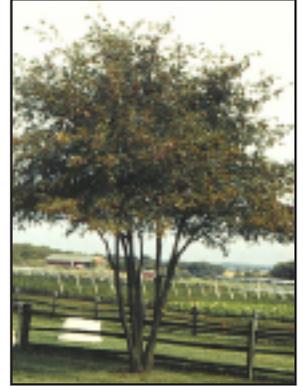
Flowering Trees



Vlx: Amelanchier canadensis



Vly: Cercis canadensis Redbud



Vlz: Crataegus phaenopyrum Hawthorn



Vlaa: Lagerstroemia indica Crepe Myrtle



Vlab: Magnolia 'Galaxy' Galaxy Magnolia



Vlac: Prunus 'Okame' Okame Cherry

Flowering Trees should be used for open space enhancements.

Appendix

A. Review Process

National Historic Trust for Preservation

District Department of Transportation

Guidelines Advisory Committee: review and give advice on the development of the Conceptual Streetscape guidelines and funding options.

DC Office of Planning

Washington Convention Center

Department of Parks and Recreation

Shaw Main Streets

Community Outreach - Public Meetings

Public Meeting #1 Information Gathering

Public Meeting #2 Presentation

Shaw Main Streets Meeting

ANC 2F Meeting

ANC 2C Meeting

East Central Civic Association Public Meeting

Friends of Kennedy Playground Meeting

Blagden Alley/ Naylor Court Association

B. Crime Prevention Through Environmental Design Considerations

Open Space Designs

Crime Prevention Through Environmental Design is an approach to design that focuses on creating spaces that reduce the risk of providing criminal opportunity. In general CPTED offering the following principles:

- Territoriality/Ownership—defines the intended use of the area? What behavior is allowed? What risks can be anticipated and planned for? Fences, pavement treatments, art, signs, good maintenance, and landscaping are some physical ways to express ownership and encourage people to protect the spaces.
- Territory—defines the physical limits of the area or public spaces.
- Design—Does the physical environment support the intended use safely and efficiently?
- Activity Support—encourage legitimate activity in public spaces to help discourage crime and increases active natural surveillance and the feeling of ownership.
- Natural surveillance—Organize circulation patterns, site features and furnishing and activities in way that maximizes the ability to see what's going on and discourages crime.
- Eliminate barriers such as dense vegetation, blind corners, shadows and poorly lit areas, and expansive spaces void of entrances or windows that reduce opportunities of spontaneous observation.
- Access control—Properly locate entrances, exits, fencing landscaping and lighting to direct both foot and automobile traffic in ways that discourage crime.

C. Web References

- Historic Preservation Guidelines
- Thrive: A Guide to Storefront Design
- DDOT Plans and Guidelines
- Convention Center Area Strategic Development Plan
- Shaw Main Streets Design Committee Work Plan
- NTHP Commercial Corridor Development Plan (7th and 9th Streets)
- SNAPS Strategic Neighborhood Action Plans
- Downtown Action Plan
- Uptown Destination District Strategic Development Plan

VI. Appendix

Plans Affecting Outside the Immediate Study Streets and Surrounding Area

- Mount Vernon Master Plan
- K Street Redevelopment Plan
- City Vista

CPTED Principles National Crime Prevention Council <http://www.ncpc.org/ncpc/ncpc>

D. Image Credits

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- Id: Cultural Tourism for DC
- IIa - IIb: Washington Historical Society
- VI f-VI h: Landscape Forms - Outdoor Furniture and Accessories
- VI o: Grow Native - Missouri Department of Agriculture
- VI p: JR Manhart- <http://www.csd.tamu.edu/FLORA/imaxxsap.htm>
- VI q: <http://www.mytho-fleurs.com>
- VI r: Cornell University Dept. of Horticulture
- VI t: University of Missouri Division of Plant Science
- VI u: Bold Spring Nursery - www.boldspring.com
- VI v: Texarkana College Biological -Sciences <http://www.texarkanacollege.edu/~mstorey/plants/P000256.jpg>
- VI w - VI x: University of Connecticut Horticulture
- VI y: "Dirr's Hardy Trees and Shrubs", Michael A. Dirr, 1997
- VI z: Tao Herb Farm- http://www.naturehills.com/Product_images/otherimages/washington_hawthorne_4.jpg
- VI a a:
- VI a b: Crocus - http://www.crocus.co.uk/graphics/products2/PL/00/00/00/41/PL0000004123_card.jpg
- VI a c: University of Alabama at Huntsville <http://www.uah.edu/admin/Fac/images/okamefl.jpg>

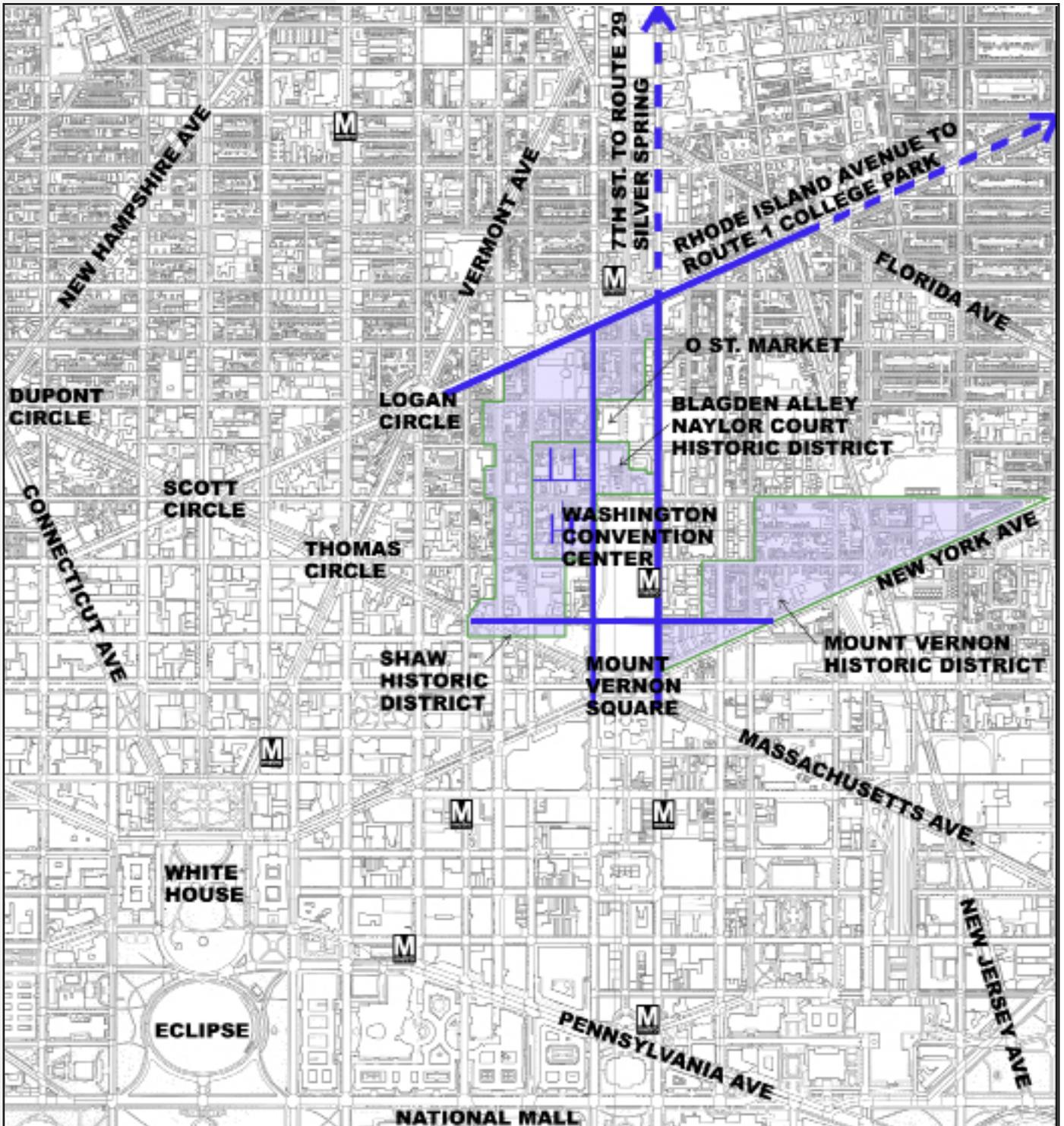


Figure VIa: Downtown Vicinity Map showing the relationship of location of the study area in respect to the rest of downtown . Connections to Silver Spring, MD and College Park, MD show regional vehicular connections.

VI. Appendix

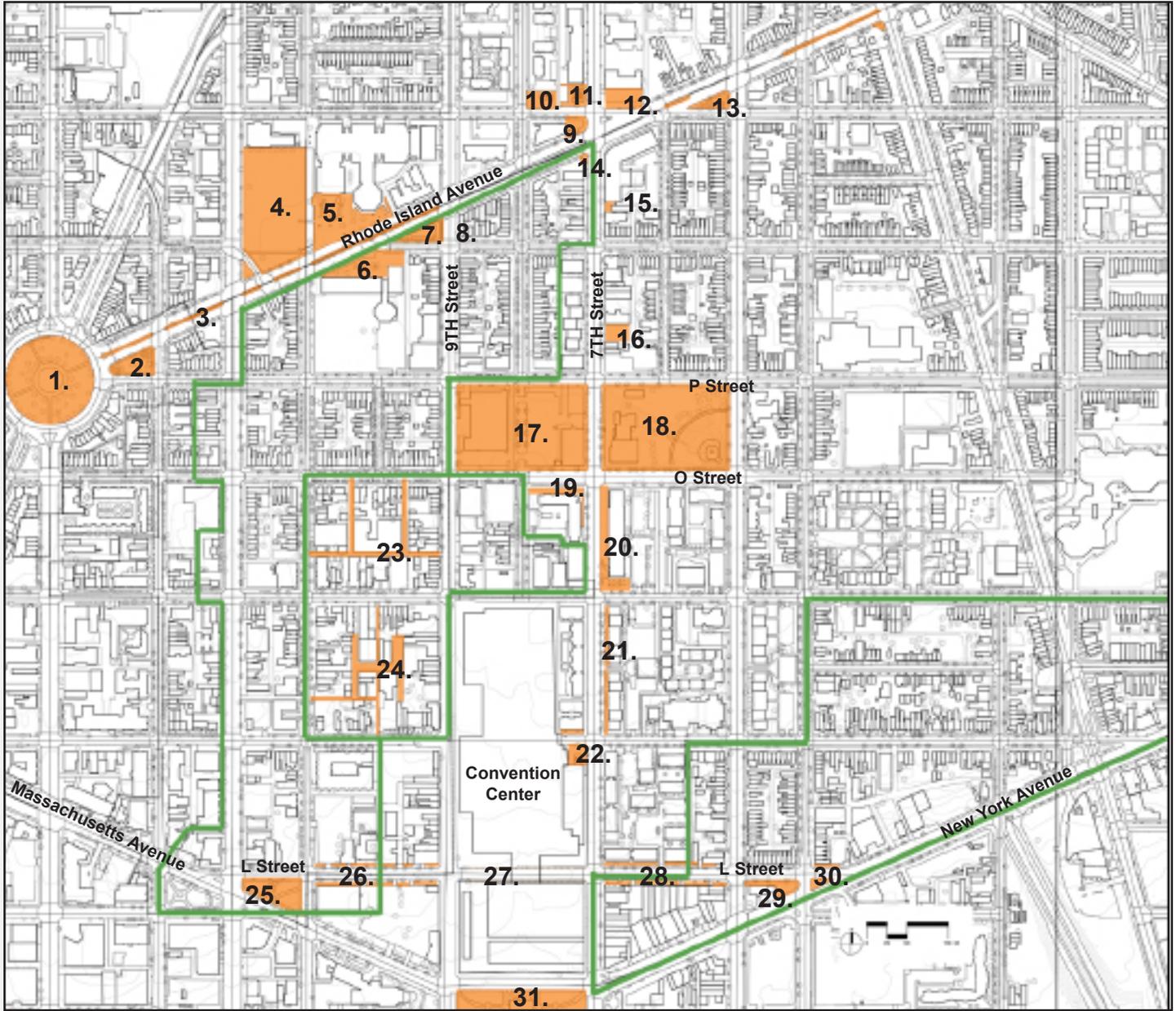


Figure VIb: Opportunity areas are highlighted to show potential for open space enhancements, redevelopment opportunities and proposed art sites

- | | |
|---|---|
| 1. Logan Circle | 16. Community Garden |
| 2. Triangle Park | 17. O Street Market/Giant Site |
| 3. Rhode Island Ave Center Median | 18. Kennedy Playground & Rec Center |
| 4. Skate Park & Ball Fields | 19. Apartment Plaza |
| 5. Shaw Jr. High School | 20. Apartment Plaza |
| 6. Seton Elementary School | 21. Building set back - Mixed use space |
| 7. Carter G. Woodson Park | 22. Convention Center Metro entrance |
| 8. S.E. corner 9th Str. & Rhode Island Ave. | 23. Naylor Court |
| 9. Watha T. Daniel Library | 24. Blagden Alley |
| 10. Shaw Metro Entrance | 25. Triangle Park |
| 11. Apartment Plaza | 26. Open Space set backs |
| 12. Plaza | 27. Convention Center bollards |
| 13. Mother Cooper Park | 28. Retail & Residential building setback |
| 14. 7/11 Parking Lot | 29. Police Station parking lot |
| 15. Courtyard | 30. Triangle median |